



**City of Biddeford
Historic Preservation Commission**

December 10, 2025 at 4:00 PM
City Hall Council Chambers & Zoom

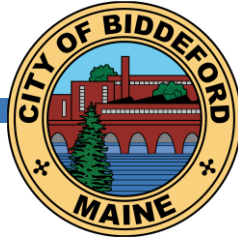
[Join Zoom Meeting Online](#)

Or call in by phone: +1 312 626 6799

Meeting ID: 950 4356 2174

Passcode: 011116

1. Declaration of Quorum/Voting Members
2. Adjustment(s) to Agenda
3. Approval of Meeting Minutes
 - 3.a Approval of Meeting Minutes from November 12, 2025
4. New Business
 - 4.a **2025.37 HPC Review** Archetype Architects to revise the design of a previously approved project at 6 and 14 Main Street, Tax Map 71, Lot 10/1 and Tax Map 71 Lot 10/4, respectively, in the MSRD-3 Zone.
5. Unfinished Business
6. Other Business
7. Adjourn



**HISTORIC PRESERVATION
MEETING MINUTES
November 12, 2025
DRAFT**

1. Declaration of Quorum/Voting Members

Meeting brought to order at 4:00 PM

Julie Larry, Erin Ware, Leah Schaffer, Shannon Chisholm

2. Adjustment to Agenda-None

3. Approval of Meeting Minutes

Approval of minutes from September 10, 2025

MOTIONS: 4:02

Motion- Larry- Motion to approve minutes as presented

Second- Chisholm

Vote-Motion passed unanimously

4. New Business

4.a. 2025.29 HPC Review Kris Samaras to install select new replacement windows at 247 Elm Street, Tax Map 34, Lot 43 in the MSRD-2 Zone.

- Kris Samaras represented the application.
- Renovating entire building downstairs has 9 windows to be replaced by double hung windows
- Removing 2 windows on basement.
- Adding small apartment to basement
- Replacing 2 windows in basement facing Mason St.
- Current condition of windows on Mason St. same as on Elm St. 6/6 screwed on poor condition with filler panel at the head
- Schaffer-they are windows without frames
- Chisholm will new windows fill entire space or will the molding be changed
- Samaras said those are the two windows to be removed and not replaced, siding over that side for a kitchenette.
- Ware-will it be clapboard, yes same as what is on there now.
- Will the siding be staggered so that it doesn't look patched? Yes
- Schaffer hard to know what the originals were
- Ware- given the asymmetrical-ness of the elevation, it's not a big deal but it would be nice to have natural light.

- Applicant would keep them if he could, but the area gets natural light from other windows. He thinks it will be fine.
- Schaffer-any changes to casement windows for egress. Once there are new windows they will be egress.
- Chisholm asked about the profile of the windows the replacement will not match the original windows that will be retained. How much visual issues will that cause. There is not much difference. She wants to make sure they are not visually off.
- Larry- It's just the 3 on the front people in cars will not notice but on the street you would have to study it. No difference to the untrained eye.
- Schaffer-the trim pieces are staying on the triple unit on the front, staying the same.
- Chisholm-No grill patterns on the new windows-correct

MOTIONS: 4:17 PM

Motion-Larry- Motion to approve Certificate of Appropriateness for Kris Samaras to replace seven windows at 247 Elm Street as presented, based on materials and assertions submitted, and conditioned on the following: All permits must be obtained from Code Enforcement prior to beginning any work.

Second-Ware

Vote-Motion passed unanimously

5. Other Business

5.a. 2025 CLG Grant Award for Additional Survey Work

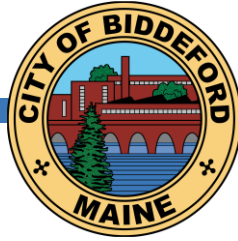
- **Grant was approved but had to be reduced from \$100,000 to \$75,000.**

6. Adjourn 4:34 PM

Chair: Historic Preservation Commission

Date

These minutes are a summary and are not intended to be verbatim. Archived meetings are viewable at the City's website: www.biddefordmaine.org



HISTORIC PRESERVATION COMMISSION REPORT

TO: The Biddeford Historic Preservation Commission (HPC)

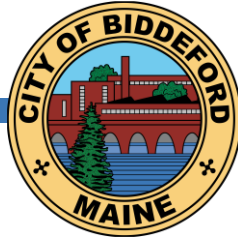
FROM: Brad Favreau, Economic Development Coordinator

MEETING DATE: Wednesday December 10, 2025, 4 PM

RE: **Item 4.1: 2025.37 HPC Review** Archetype Architects to revise the design of a previously approved project at 6 and 14 Main Street, Tax Map 71, Lot 10/1 and Tax Map 71 Lot 10/4, respectively, in the MSRD-3 Zone.

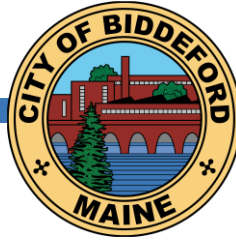
1. INTRODUCTION

The Historic Preservation Commission reviewed and approved this project to renovate Buildings 19 and 20 for residential use on January 8, 2025. The owner and designers now propose changes to the original application that are substantial enough to require a second review.



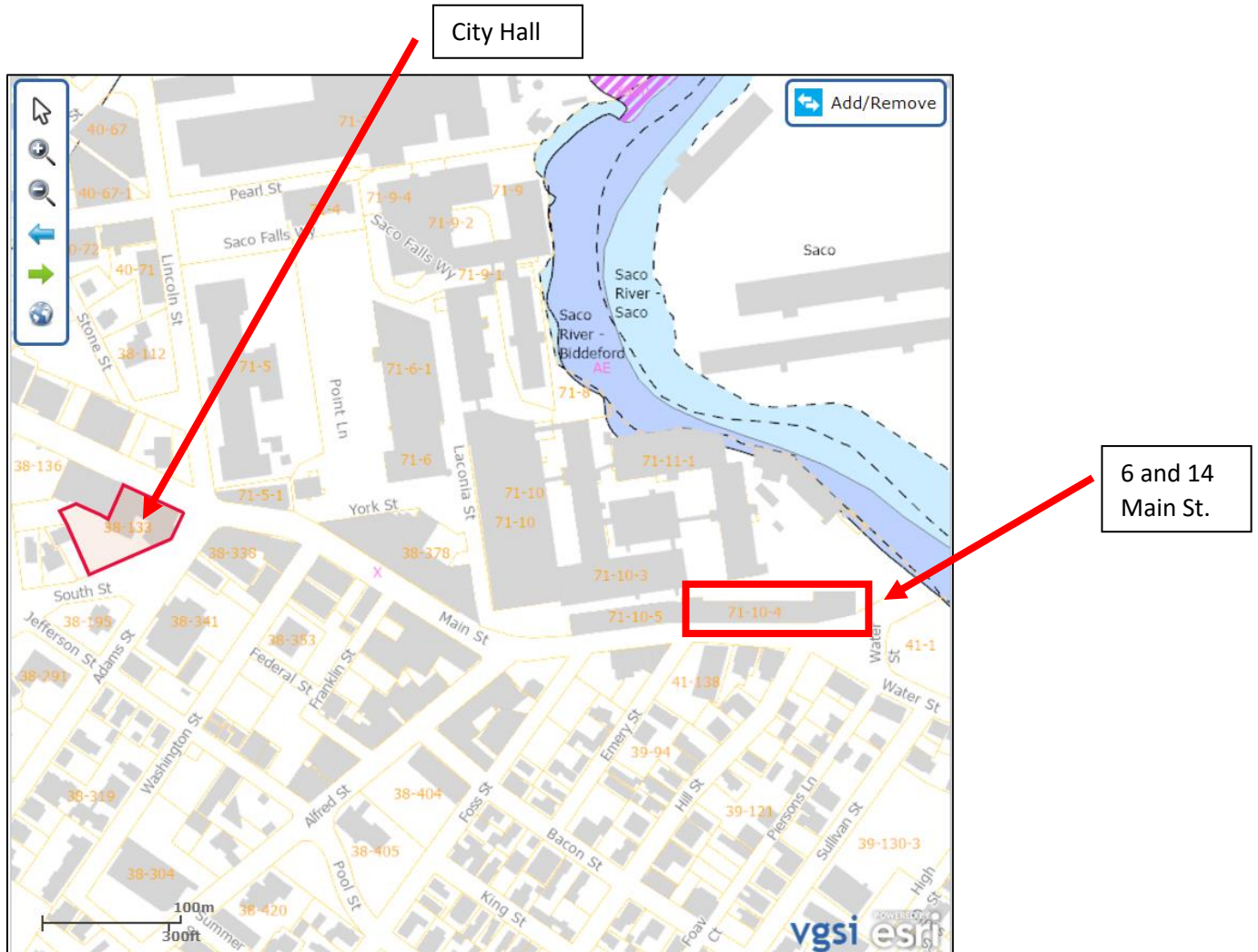
2. PROJECT DATA/INFORMATION

	SUBJECT	DATA/INFORMATION
1.	Applicant:	1921 Pepperell LLC 72 Commercial St Suite 2 Portland ME 04101
2.	Owner of Property:	1921 Pepperell LLC
3.	Agent:	N/A
4.	Engineer/Architect:	Archetype, Portland, ME
5.	Project Location:	6 Main Street, 14 Main Street Buildings 20 and 19
6.	Project Tax Map #/Lot #:	Tax Map 71, Lot 10-4; Map 71, Lot 10-1
7.	Existing Zoning:	MSRD-3
8.	Overlay Zoning:	Biddeford / Saco Mills Historic District
9.	Contributing?	Yes
10.	National Register of Historic Places?	Yes
11.	Approximate Date of Construction	Circa 1890
12.	Existing Use:	Vacant
13.	Proposed Use:	Residential
14.	Uses in the Vicinity:	Mixed Use
15.	Parcel Size:	0.46 acres (combined)
16.	Front Setback Required:	None
17.	Side Setbacks Required:	None
18.	Rear Setback Requires:	None
19.	Height Requirements:	Min. 2 stories or 26 feet
20.	LDR Attachment A: Fess Paid:	Yes
21.	Historic Preservation Commission Review History:	Meeting date Dec. 10, 2025. Posted Dec. 3, 2025. Mail Notices to all abutters within 100'. 3 notices sent Dec. 3, 2025.

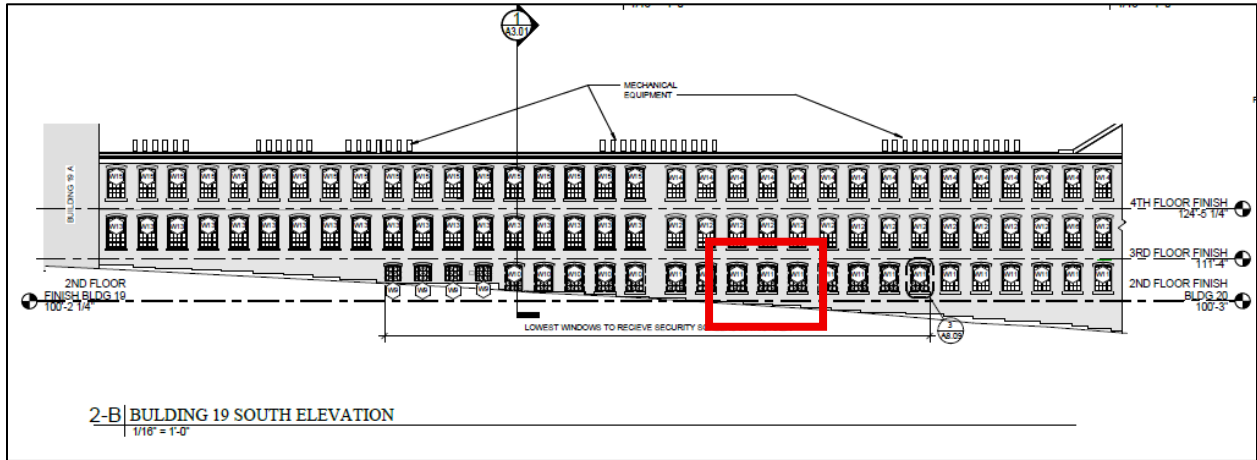
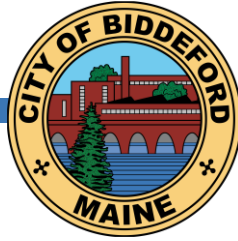


3. EXISTING CONDITIONS

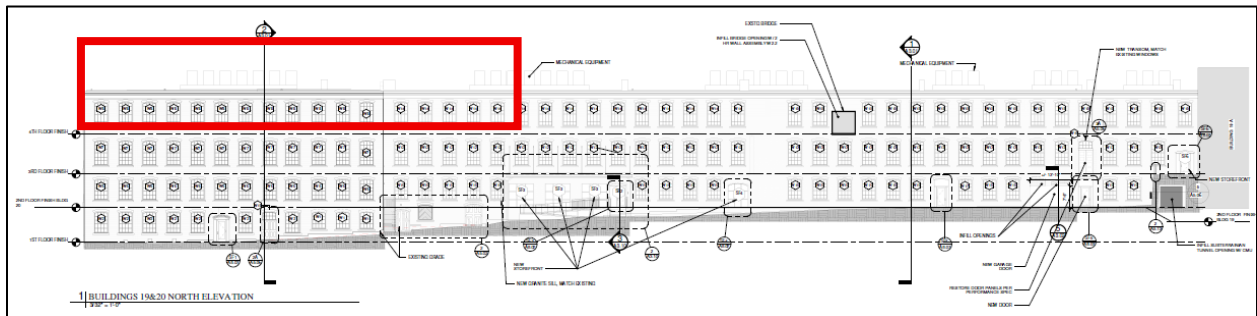
The subject property is located at the lower end of Main Street, adjacent to the entrance to North Dam Mill complex. These buildings are highly visible at the lower end of Main Street and are **Contributing** to the Biddeford / Saco Mills Historic District under the areas of Industry and Architecture.



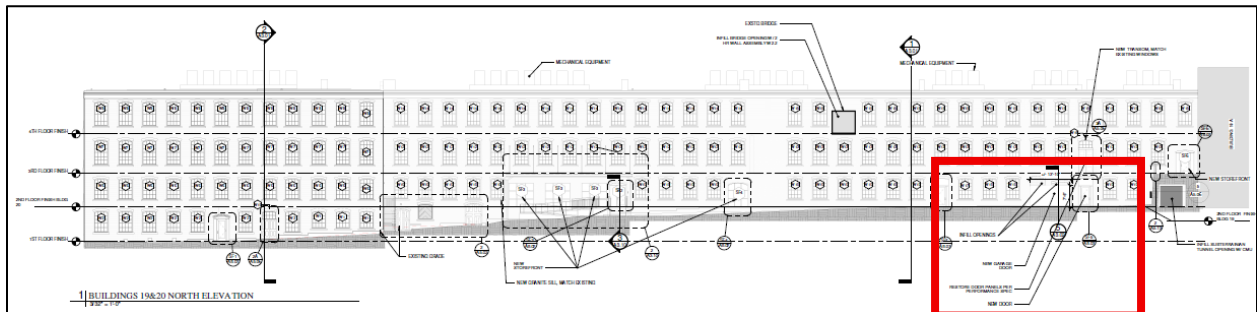
Source: "14 Main Street." [Vision Government Solutions](http://www.visiongovernment.com).
Accessed Nov. 29, 2024.

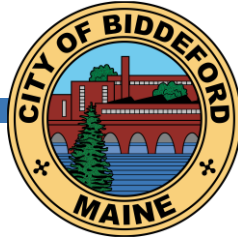


Revisions are shown below. Roof deck and railing, and roof access (stairs, elevator) have been deleted from the design. Other mechanical equipment will remain:

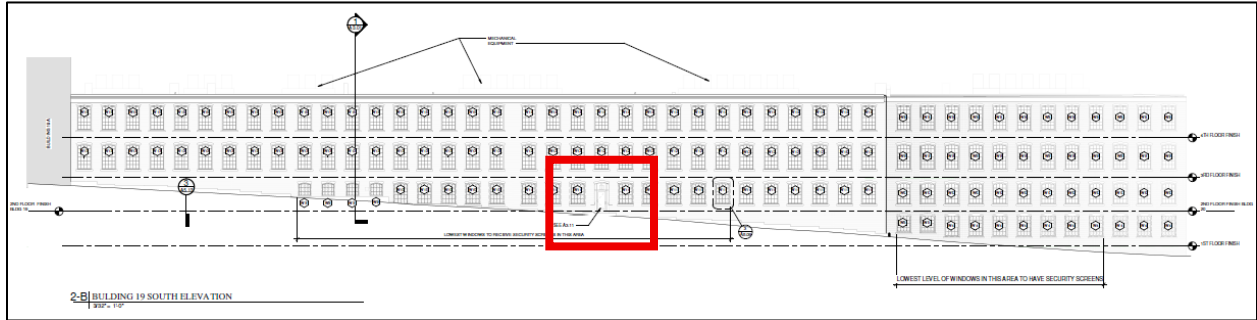


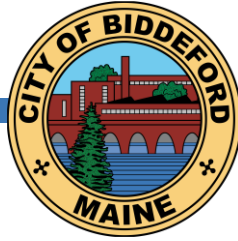
On the same elevation, a new overhead door and new egress door have been added to the design:



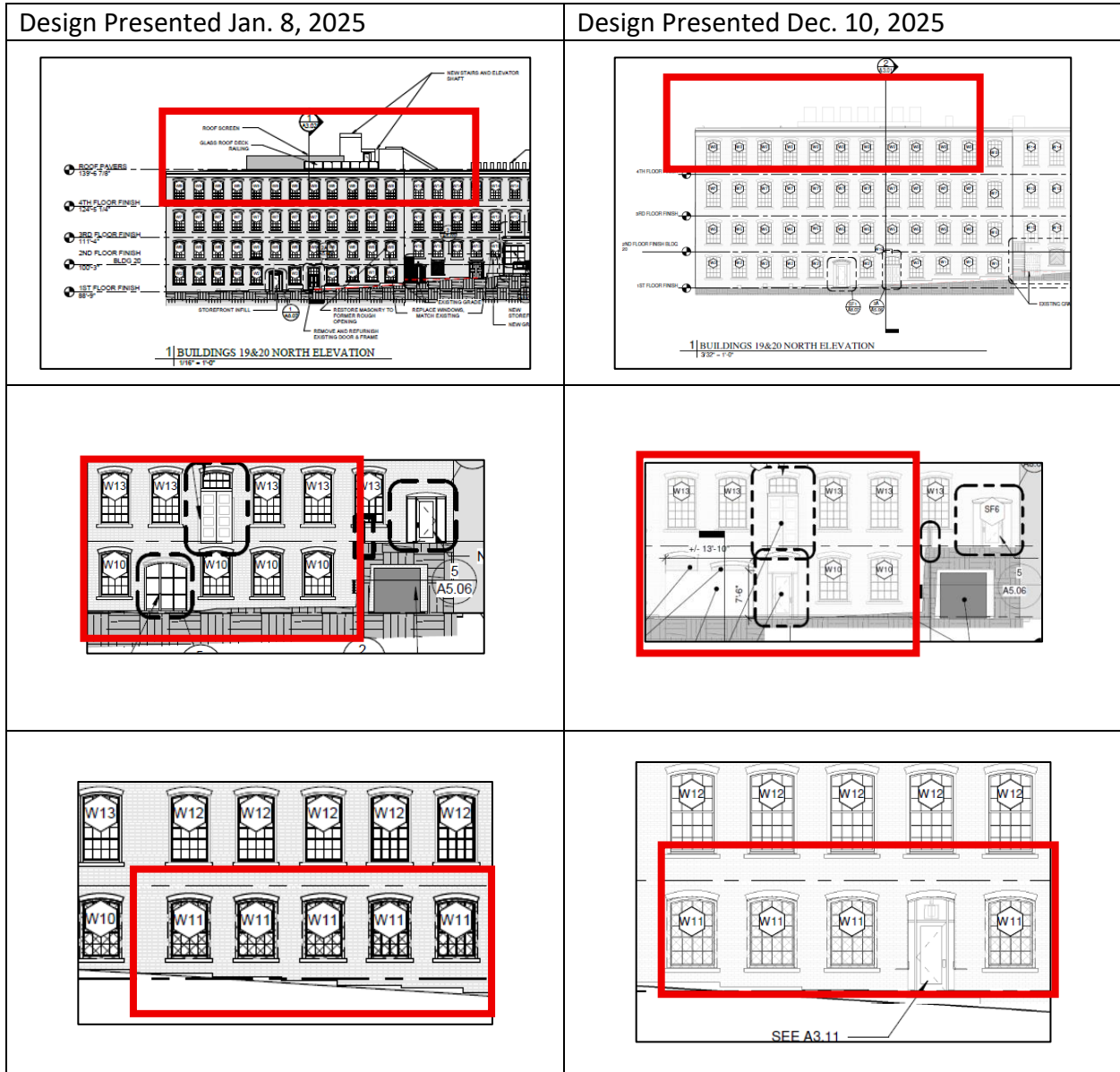


On the South elevation, an existing window opening will become a doorway:

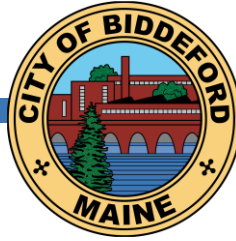




Side-by-side comparisons show the changes being presented:



See “Renderings and Products” in the agenda packet for details. The new entry on the North elevation and the new egress door on the South elevation will be of the same storefront system by Architectural Glazing Systems, Inc.



The agenda packet also includes the following *Interpreting the Standards* (ITS), published by the National Park Service, for additional information:

- ITS 29 *Adding Vehicular Entrances and Garage Doors to Historic Buildings*
- ITS 17 *Adding Parking to the Interior of Historic Buildings*
- ITS 22 *Adding New Entrances to Historic Buildings*

These bulletins include case studies that offer insight to applying Standards to a variety of preservation situations.

5. PUBLIC COMMENT

City Staff posted this notice on the city website and at City Hall on Wednesday, December 3, 2025. No comment has been received at the time of this writing.

6. STAFF REVIEW:

The applicant is seeking historic preservation tax credits for this project and is working with Heritage Consulting Group to prepare the NPS Certification Application Part 2.

The review of Standards below is based only on design changes presented at this time and does not reflect the project as presented in January 2025.

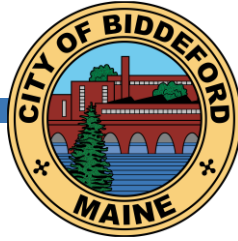
7. STANDARDS

A. REVIEW STANDARDS: Article XV (Historic Preservation Ordinance),

Section 13. Evaluation standards for certificate of appropriateness.

A. Reconstruction and alterations.

1. A building or structure classified as an historic landmark or located within an historic district, or any part thereof, or any appurtenance related to such structures, including but not limited to walls, fences, light fixtures, steps, paving and signs, shall not be reconstructed or altered unless a certificate of appropriateness has been issued for such activity. No certificate of appropriateness for a structure or building identified as contributing to the district shall be issued unless the proposed activity is found to preserve or enhance a building's historical or architectural character. No certificate of appropriateness shall be issued for a noncontributing structure or building unless the



proposed activity is complementary to the historic character of surrounding structures and buildings and meets the intent of this ordinance.

2. The standards and requirements in the United States Secretary of the Interior's Standards of Rehabilitation as well as the following factors shall be considered when reviewing applications for reconstruction or alteration of buildings or structures subject to review under this ordinance:

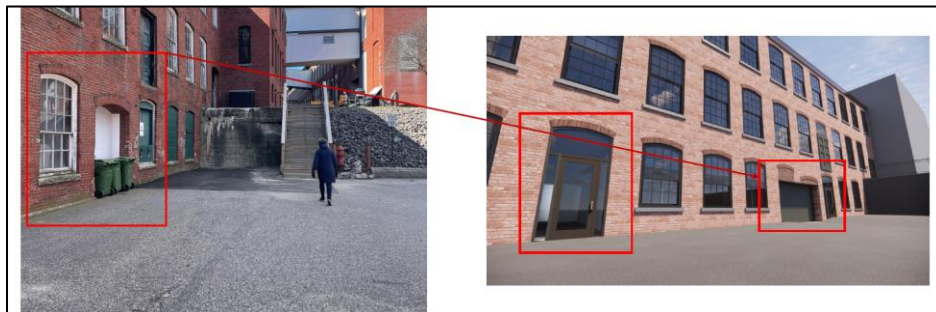
- a. Every reasonable effort shall be made to use a property for its historic purpose and in a way which will require minimum alteration to the structure and its environment;

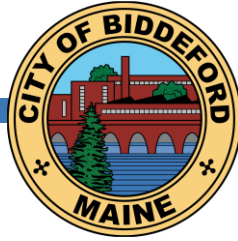
Like other buildings in the Mill District in the recent past, these buildings will be converted to residential use. The dwelling units will be market rate apartments.

- b. Rehabilitation work shall not destroy the distinguishing qualities nor character of the structure and its environment. The removal or alteration of any historic material or architectural features should be avoided;

An existing dock door and an existing window opening on the North elevation will, together, be modified to accommodate a new overhead door opening. The existing arches will be retained, with the area between the existing arch and the new opening infilled. With a new metal overhead door, this work will slightly change the character of a secondary façade of this building.

Similarly, on the South elevation, an existing window opening will be enlarged to create a doorway. This new door is simple and scaled appropriately and maintains the character of the primary façade of the building. See images below and other drawings in the agenda packet for details.





- c. Deteriorated architectural features should be repaired rather than replaced, wherever possible. In the event that replacement is necessary, the new material should match the material being replaced in composition, design, texture and other visual qualities. Repair or replacement of missing architectural features should be based on physical or pictorial evidence rather than on conjectural designs or the availability of different architectural features from other buildings;

N/A

- d. Distinctive stylistic features or examples of skilled craftsmanship which characterize historic structures and often predate the mass production of building materials shall be treated with sensitivity;

N/A

- e. Changes which may have taken place in the course of time are evidence of the history and development of the structure and its environment, and these changes shall be recognized and respected;

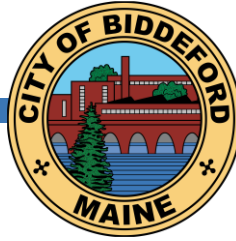
N/A

- f. All structures shall be recognized as products of their own time. Alterations to create an earlier appearance shall be discouraged;

N/A

- g. Contemporary design for additions to existing structures shall be encouraged if such design is compatible with the size, scale, material and character of the neighborhood, the structure or its environment; and,

N/A



- h. Wherever possible, new additions or alterations to structures shall be done in such a manner that if they were to be removed in the future the essential form and integrity of the original structure would be unimpaired.

N/A

8. SAMPLE MOTIONS

A. Motion to approve Certificate of Appropriateness for 1921 Pepperell LLC to revise the design of the project at 6 and 14 Main Street, first presented and approved by the commission on January 8, 2025, by removing proposed roof access from the design and by installing three new openings on two elevations, based on assertions made and materials submitted, and conditioned on the following:

1. *All permits must be obtained from Code Enforcement prior to beginning any work.*

2. _____

B. Motion to deny the Certificate of Appropriateness for 1921 Pepperell LLC to revise the design of the project at 6 and 14 Main Street, first presented and approved by the commission on January 8, 2025, by removing proposed roof access from the design and by installing three new openings on two elevations, based on the following objections:

1. _____

2. _____

C. Motion to postpone the Certificate of Appropriateness for 1921 Pepperell LLC to revise the design of the project at 6 and 14 Main Street, first presented and approved by the commission on January 8, 2025, by removing proposed roof access from the design and by installing three new openings on two elevations, based on the following:

1. _____

2. _____

ARCHETYPE ARCHITECTS

48 Union Wharf, Portland Maine 04101

207.772.6022

November 14, 2025

David Galbraith
Planning & Development
205 Main Street, Biddeford, ME 04005

Modification Request

Dear Mr. Galbraith,

On behalf of our client, 1921 Pepperell LLC., I am requesting modification of our Planning Board and Historical Board approvals of 10/9/24. I appreciate the opportunity to present, in elevation and plan, the modifications we have come up with during the construction document process. I will be referring to the plan and elevation drawings that are attached.

1. Removal of the roof deck.

The roof deck originally approved is shown on plan Dwg. A1.05 dated 10/9/24. The area of work is roughly from column lines 30 through 35 and includes Stair B, Stair C, an elevator with vestibule, the deck itself and safety railing. The elevation of these items can be seen on drawing A2.01 dated 10/9/24. See Dwg. A2.01 dated 11/14/25 for the items removed. Attached also is Dwg. D-9 dated 10/9/24 Existing Elevation for comparison. It is our intention to not construct any of these items on the roof. See attached roof Dwg. A1.06. Stair B will go from the 4th floor to exterior grade. Access to the roof will be through a roof hatch in Stair B as shown on. Other items on the roof are limited to RTU's, vents and roof drains.

2. Addition of interior parking.

We propose adding 8 interior parking spaces on the 2nd floor between column lines 2 and 10. See attached 2nd floor plan Dwg. A1.03 dated 11/14/25. Modifications to the exterior involve adding egress doors at column lines 4/5 and 10/11. In addition, we propose enlarging the opening of 5/6 to include 6/7 which would allow for a garage door. See elevation sheet 2A2.01 dated 11/14/25.

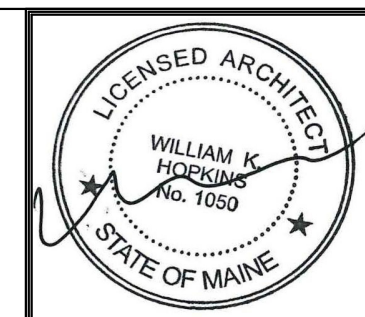
3. Adding entry on Main Street.

We propose adding a new entry of Main Street located in building 19 on the 2nd floor between column lines 20 and 21. The entry door will be recessed and will swing outward into a small entryway. There will be two risers and handrails. An exterior pendant light fixture will be installed in the recessed entryway. See Dwg A3.11 dated 11/14/2025.

If you could review these changes and get back to us with any comments, board presentations etc.

Respectfully,

Bill Hopkins
Archetype Architects



Prepared For:
1921 Pepperell, LLC

Consultant:

ARCHETYPE ARCHITECTS
48 Union Wharf Portland, Maine 04101
207.772.6022 ARCHETYPE@ARCHETYPEA.COM

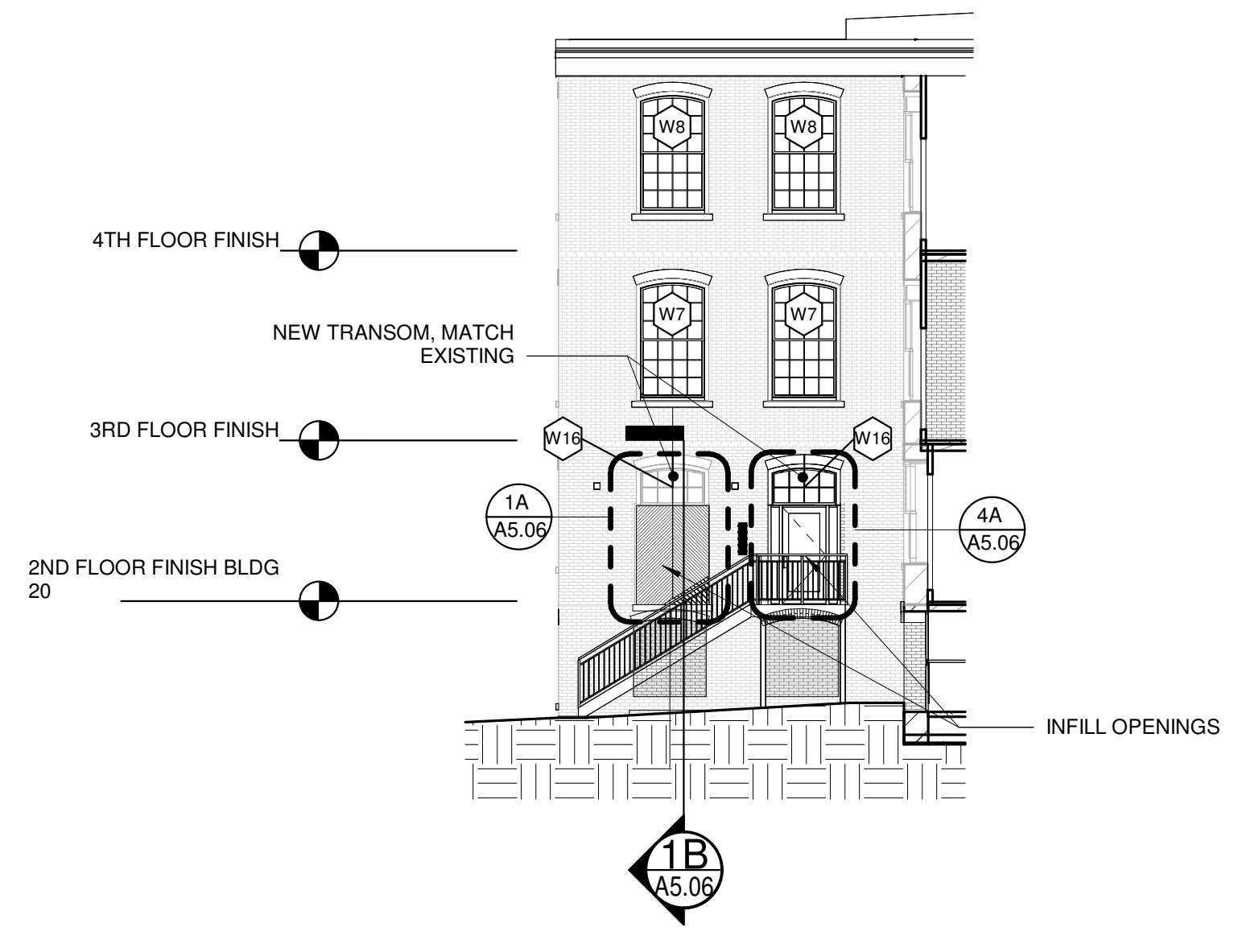
Architect:

Project:
BUILDINGS 19 & 20
PEPPERELL, BIDDEFORD, ME

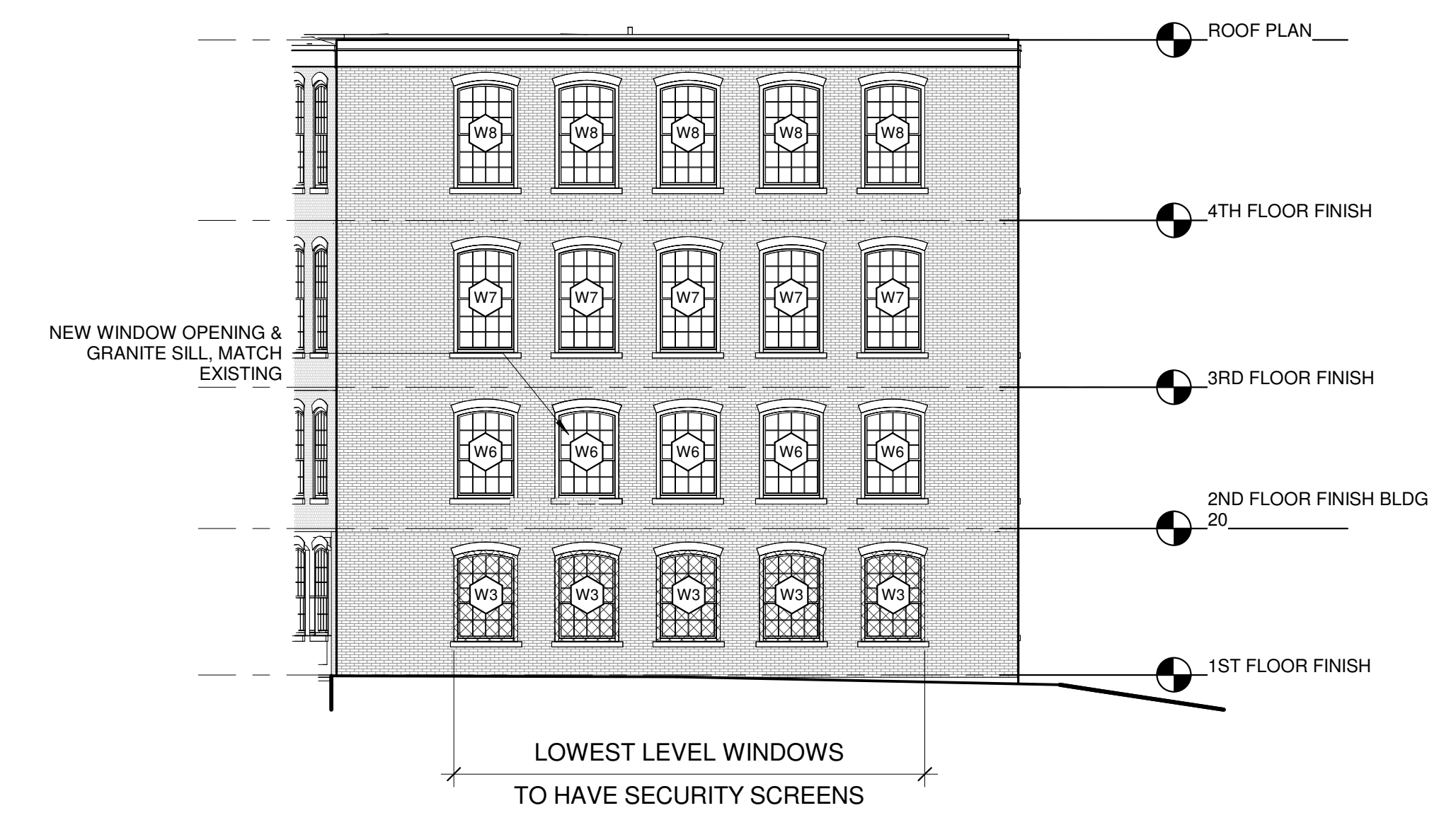
Revisions:
1 2025-10-03 9% Pricing Set
3 2025-11-14 Modification Request

Date: 11-03-2025
Scale: As indicated
BUILDING ELEVATIONS

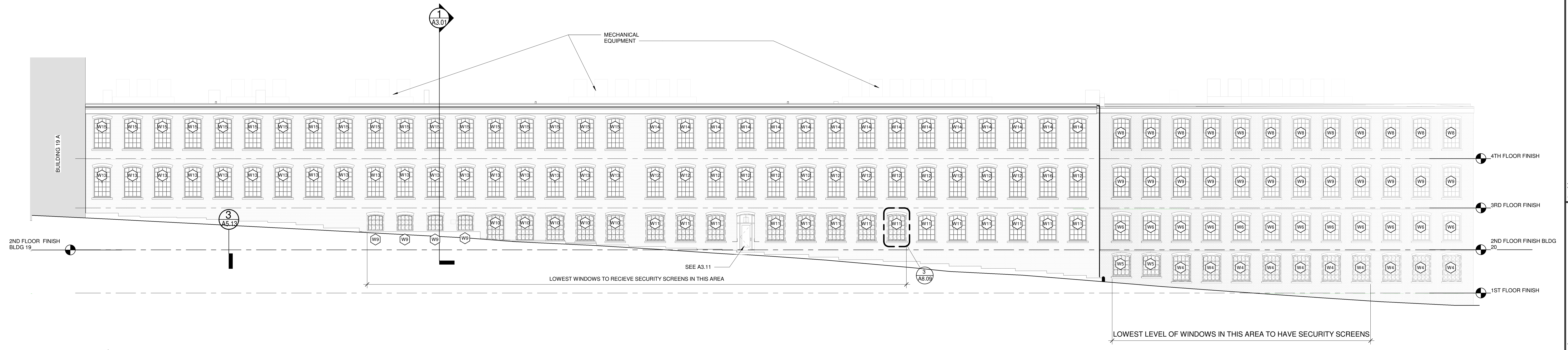
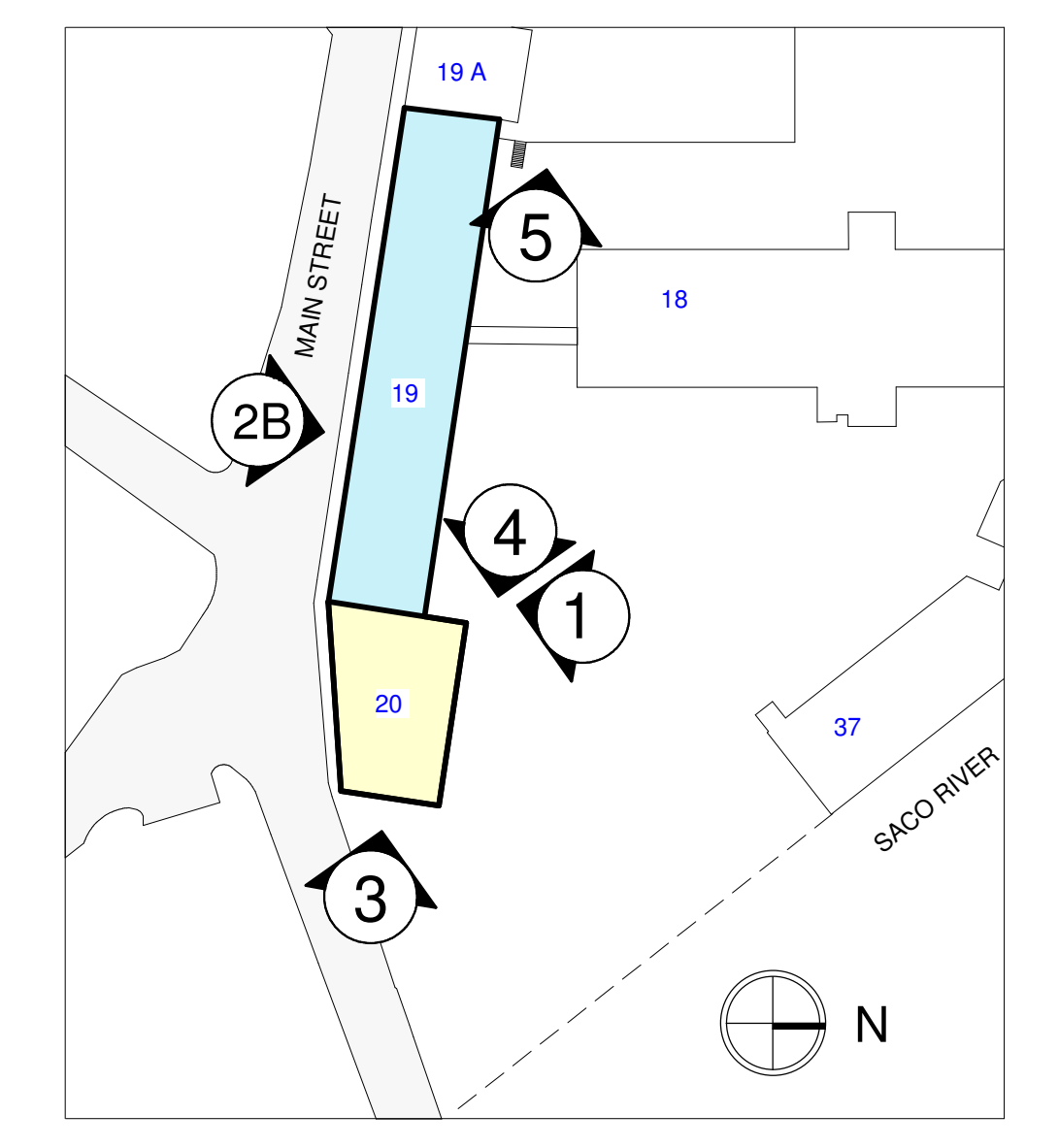
A2.01



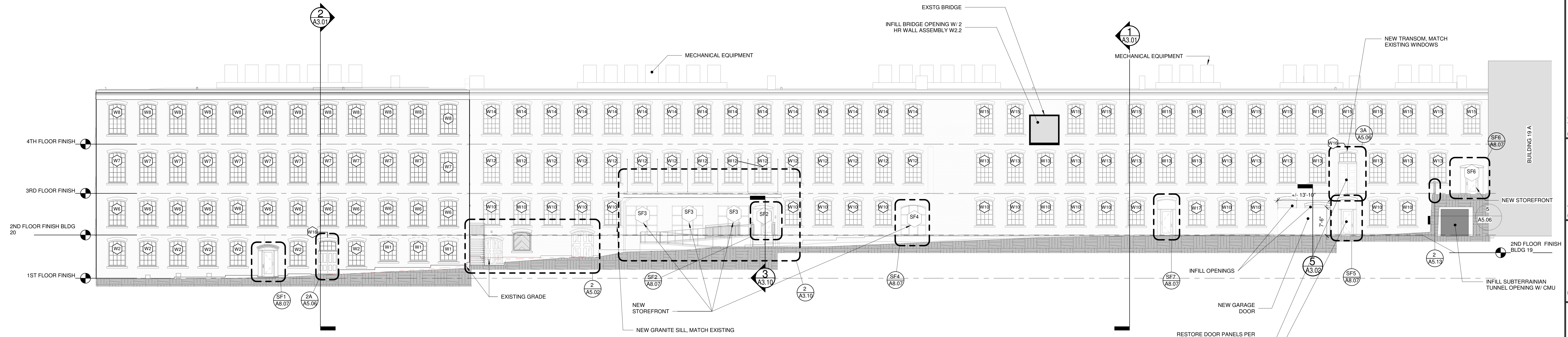
4 BUILDING 20 WEST ELEVATION
3/32" = 1'-0"



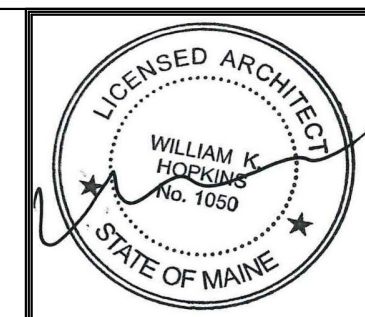
3 BUILDING 20 - EAST ELEVATION
3/32" = 1'-0"



2-B BUILDING 19 SOUTH ELEVATION
3/32" = 1'-0"



1 BUILDINGS 19&20 NORTH ELEVATION
3/32" = 1'-0"



Prepared For:
1921 Pepperell, LLC

Consultant:

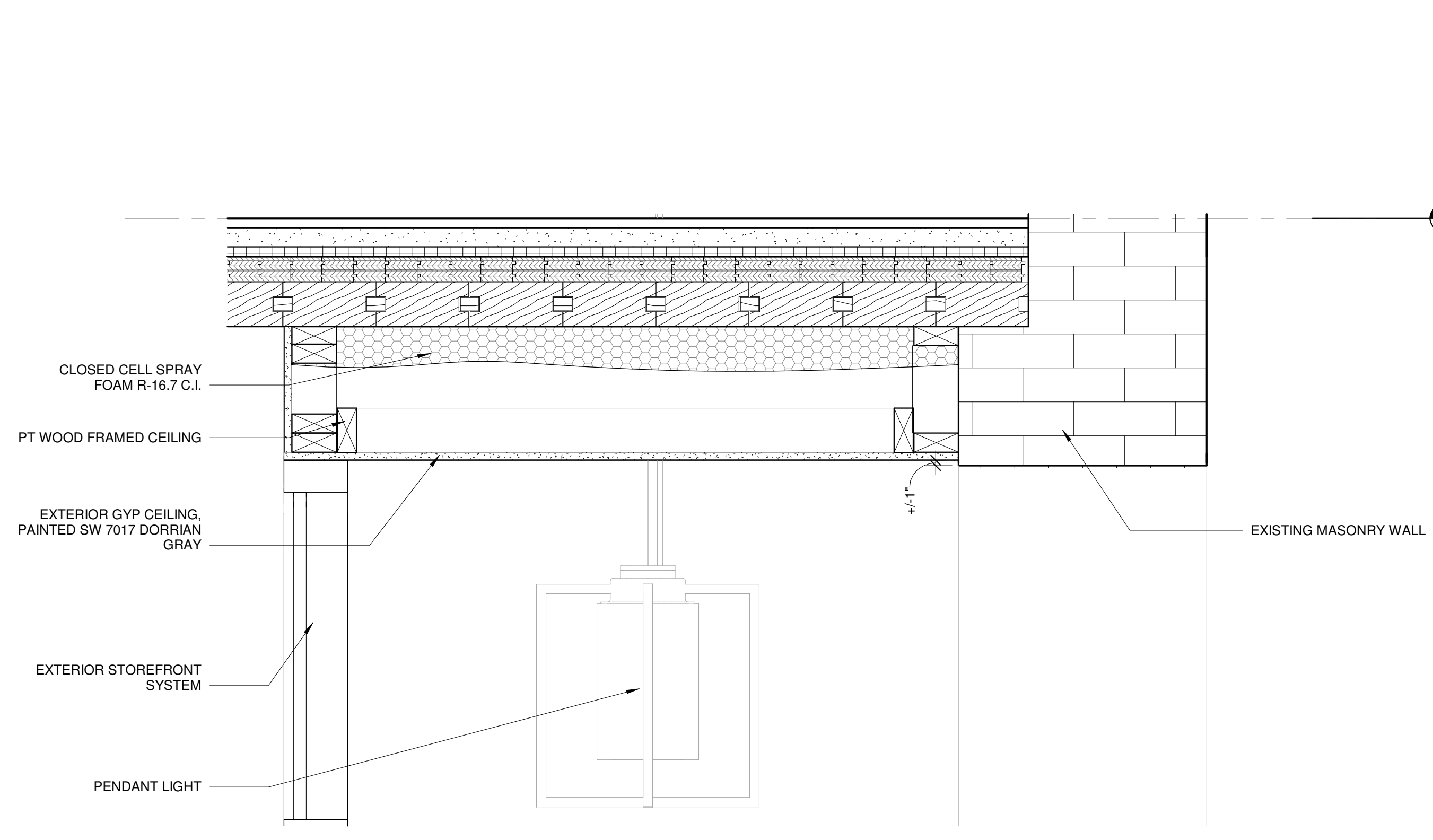
Architect:
ARCHETYPE ARCHITECTS
48 Union Wharf Portland, Maine 04101
207-772-6022 ARCHETYPE@ARCHETYPEPA.COM

Project:
BUILDINGS 19 & 20
PEPPERELL, BIDDEFORD, ME

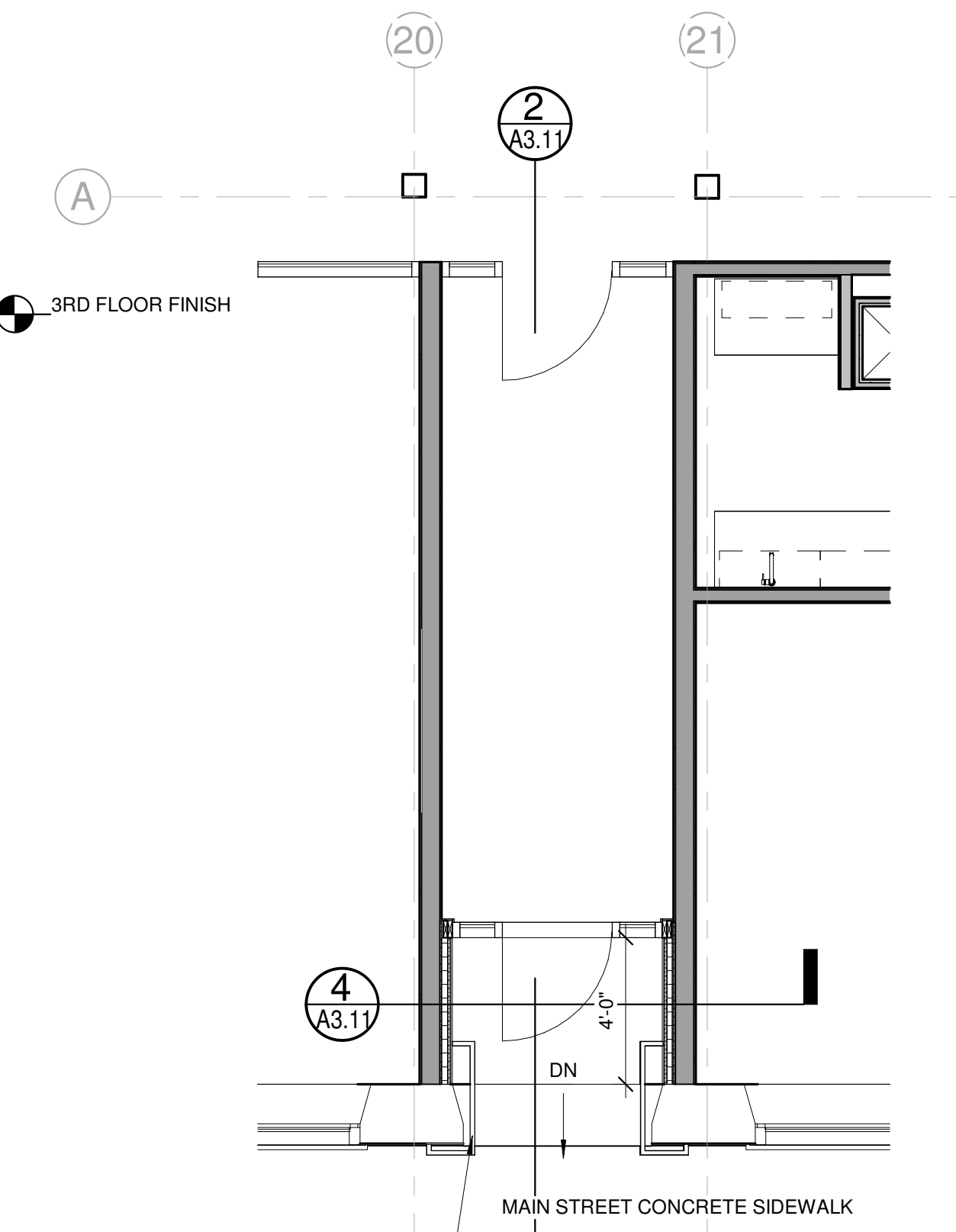
Revisions:
3 2025-11-14 Modification Request

Date: 11-03-2025
Scale: As indicated
MAIN STREET ENTRY

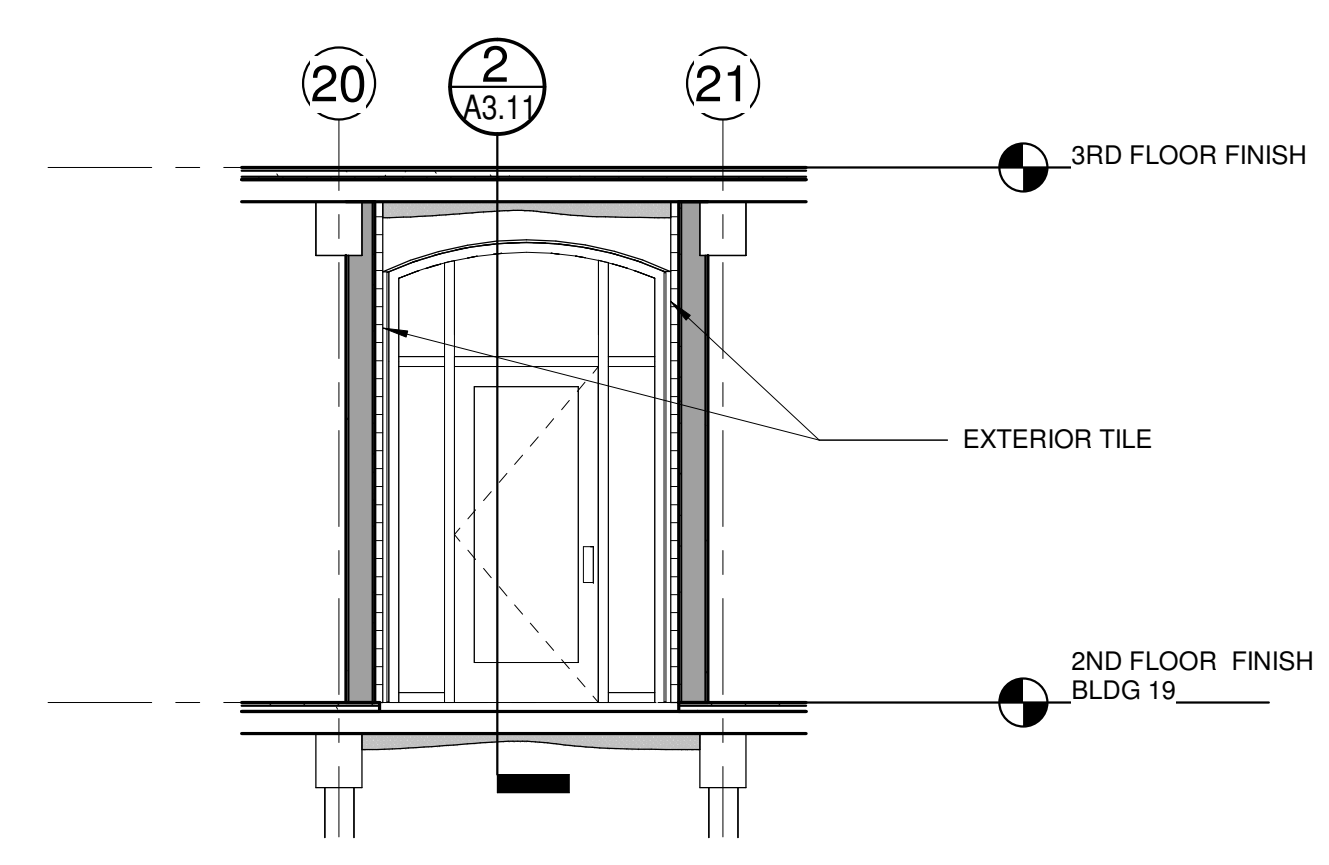
A3.11



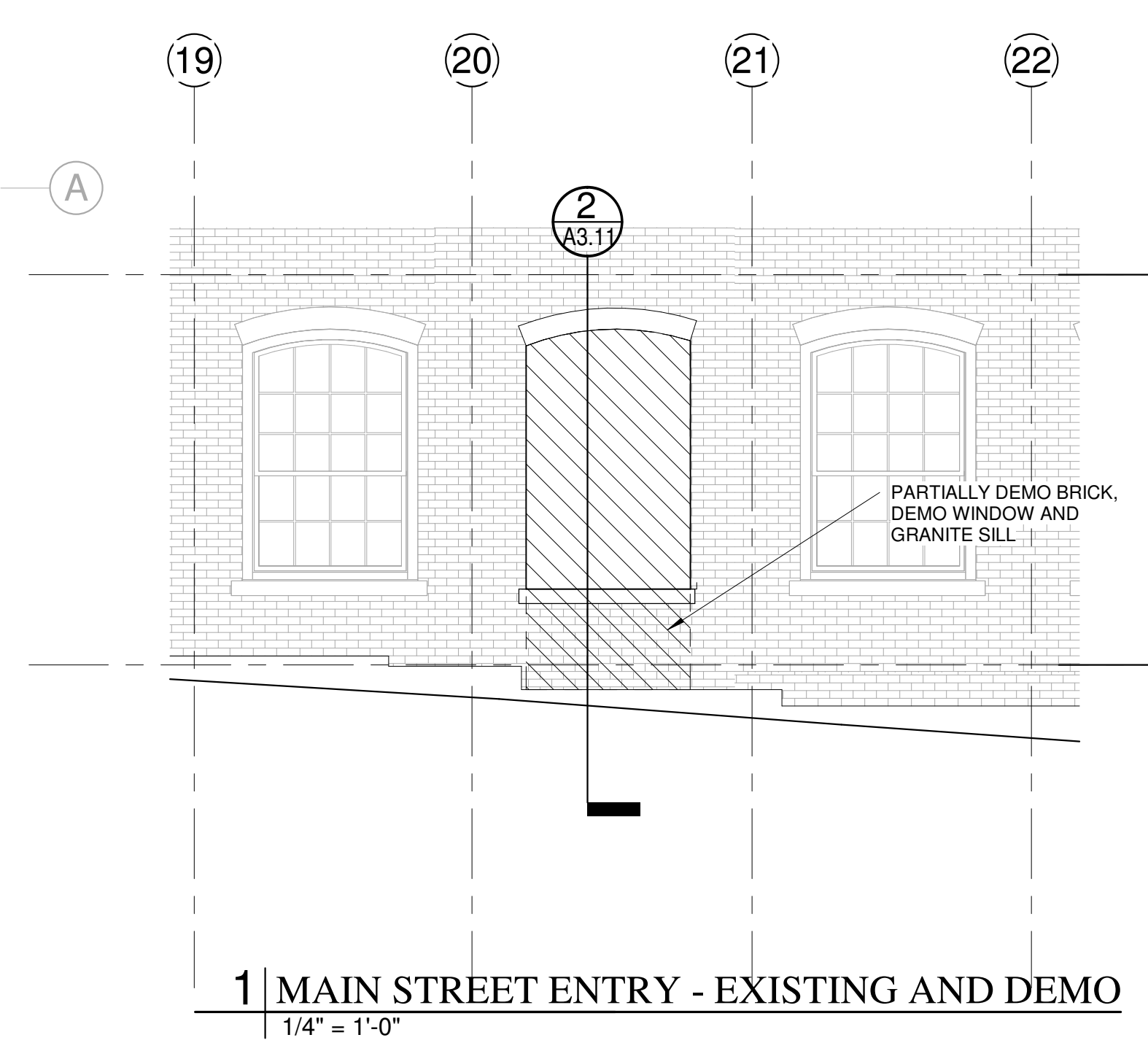
6 MAIN STREET ENTRY CEILING SECTION
1 1/2" = 1'-0"



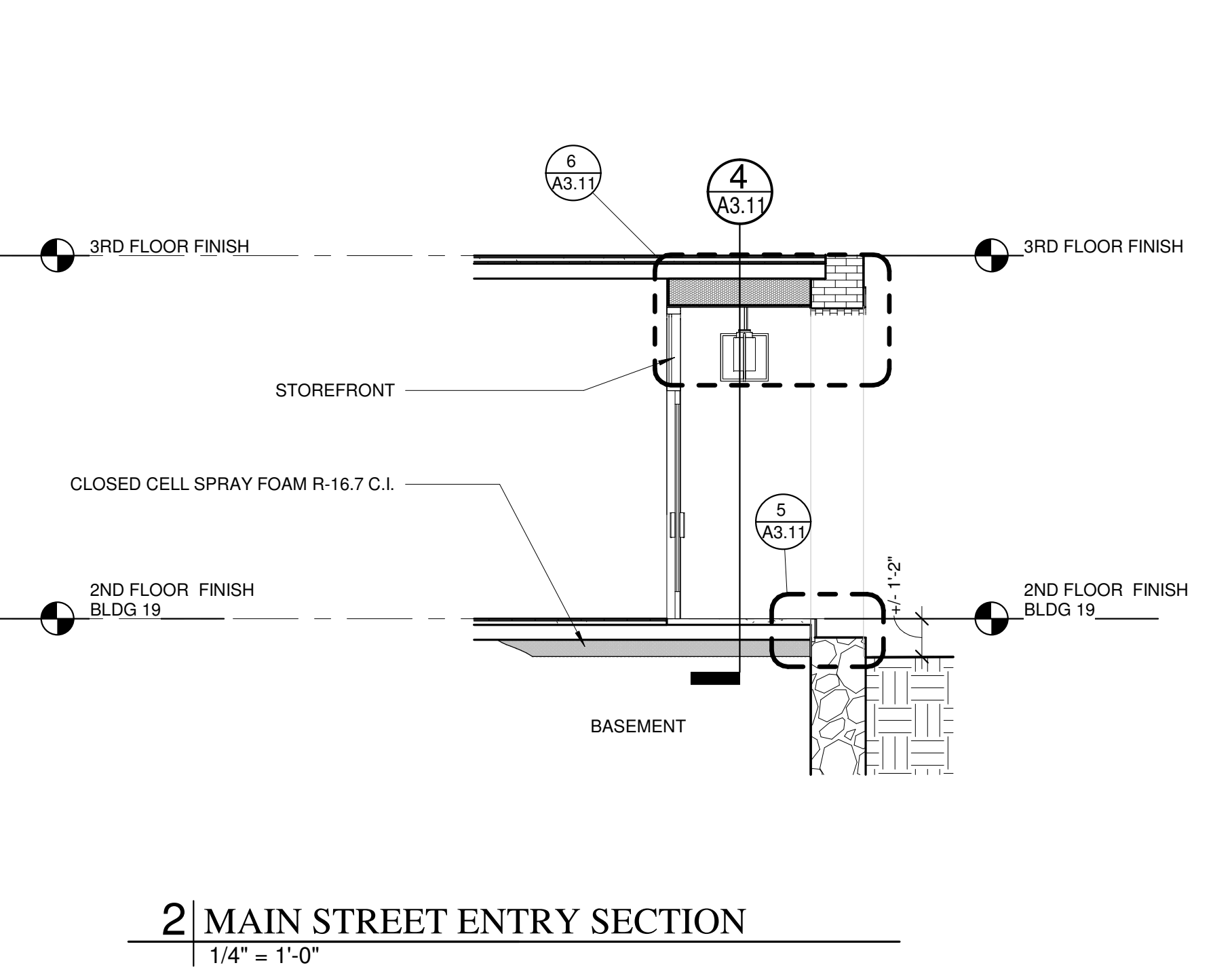
3 MAIN STREET ENTRY PLAN
1/4" = 1'-0"



4 MAIN STREET ENTRY SECTION
1/4" = 1'-0"



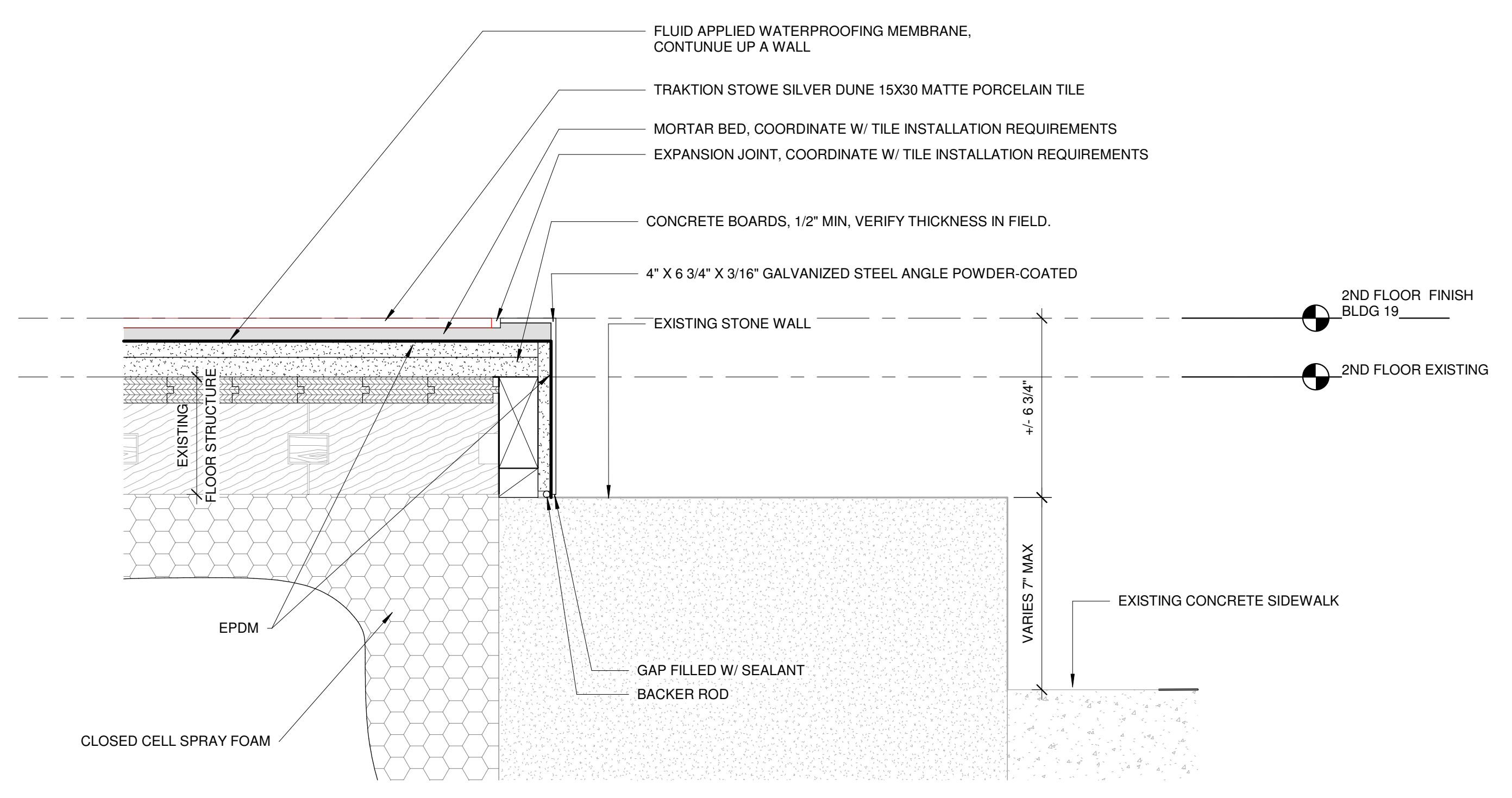
1 MAIN STREET ENTRY - EXISTING AND DEMO
1/4" = 1'-0"



2 MAIN STREET ENTRY SECTION
1/4" = 1'-0"

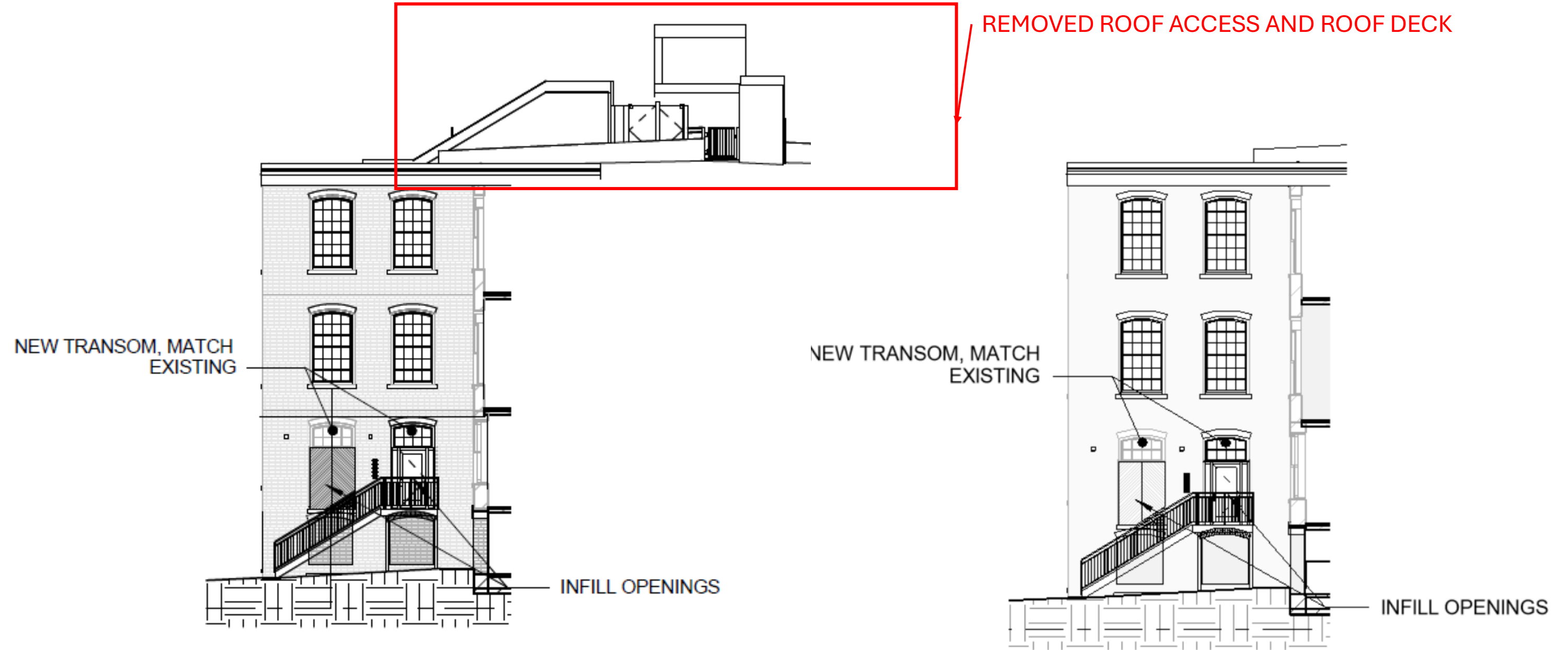


MAIN STREET ENTRY



5 MAIN STREET ENTRY STEP DETAIL
3" = 1'-0"

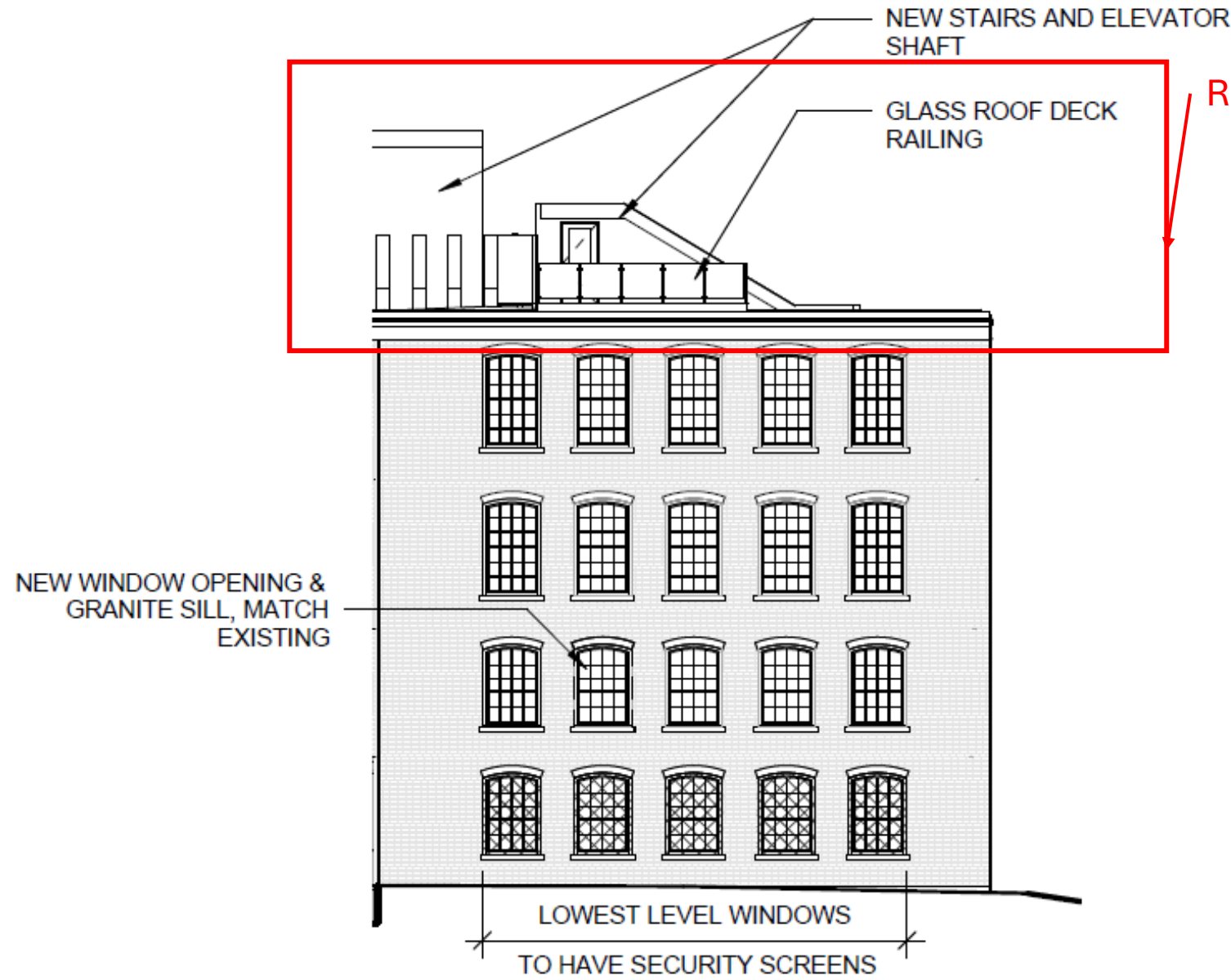
Building 20 – WEST ELEVATION



APPROVED BY PLANNING
10/9/2024

PROPOSED REVISION
11/14/2025

Building 20 – EAST ELEVATION



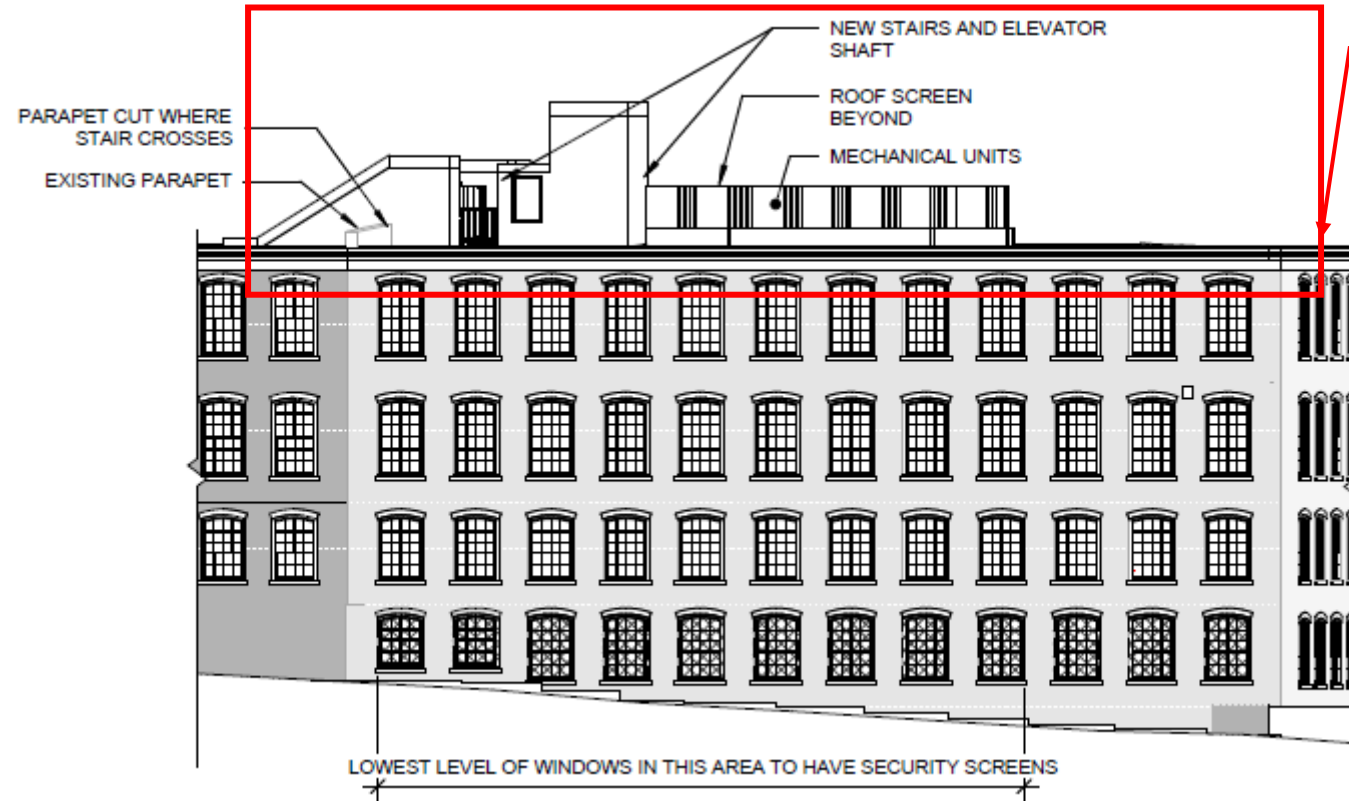
REMOVED ROOF ACCESS AND ROOF DECK



APPROVED BY PLANNING
10/9/2024

PROPOSED REVISION
11/14/2025

Building 20 – SOUTH ELEVATION



REMOVED ROOF ACCESS AND ROOF DECK

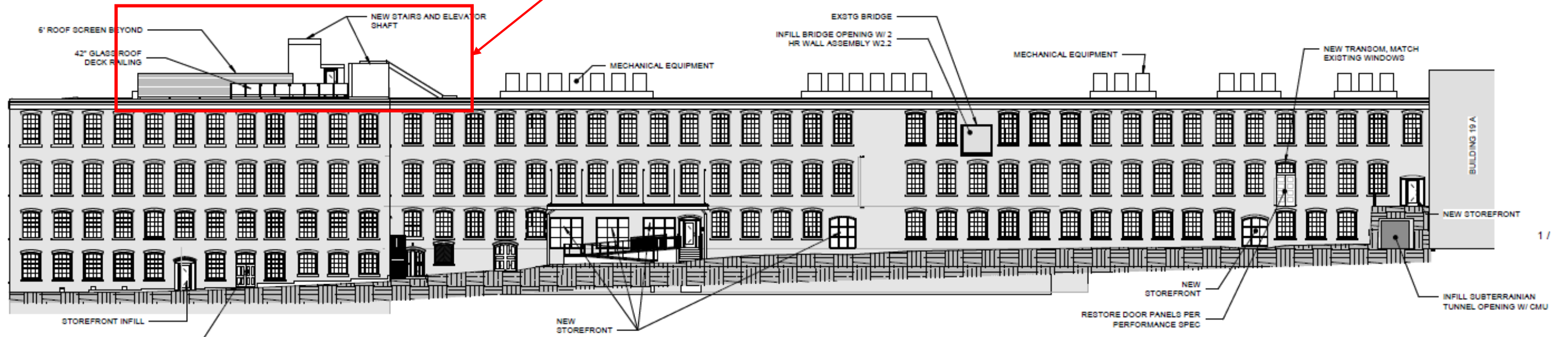


APPROVED BY PLANNING
10/9/2024

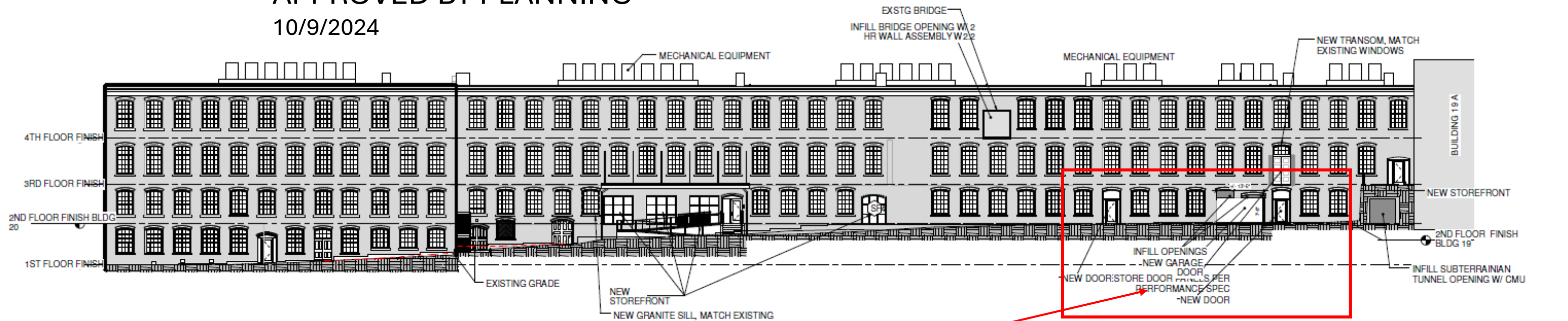
PROPOSED REVISION
11/14/2025

Building 19 & 20 – NORTH ELEVATION

REMOVED ROOF ACCESS
AND ROOF DECK

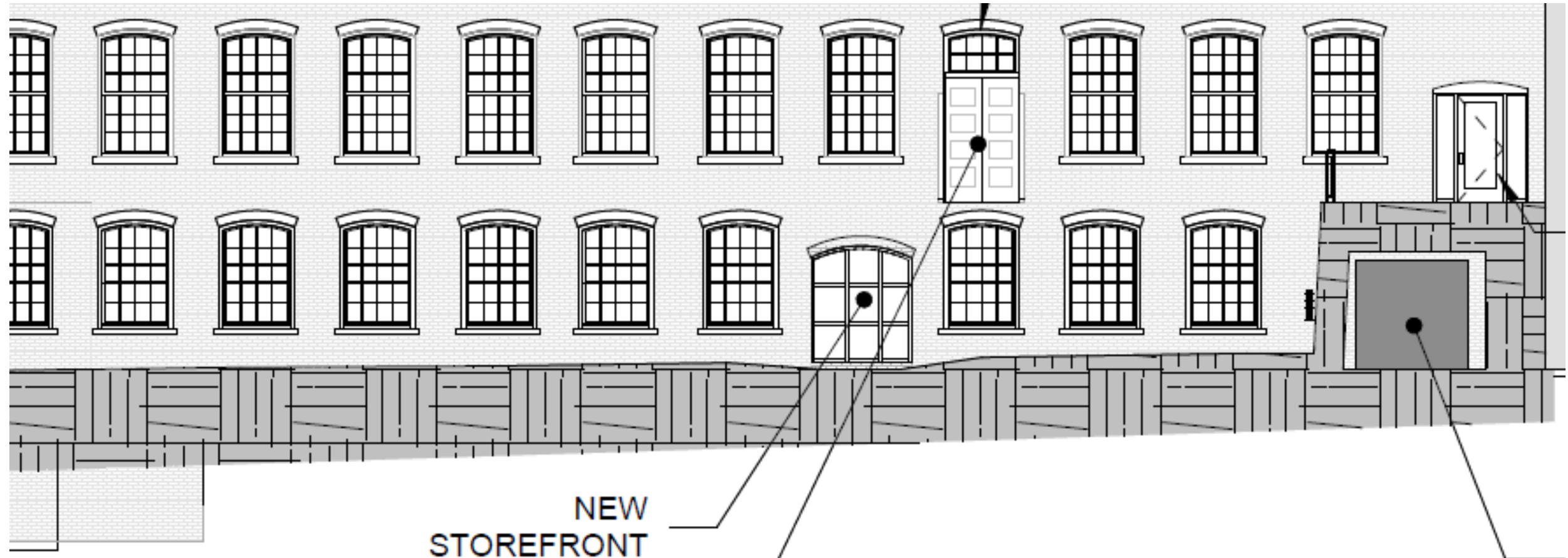


APPROVED BY PLANNING
10/9/2024



PROPOSED REVISION
11/14/2025

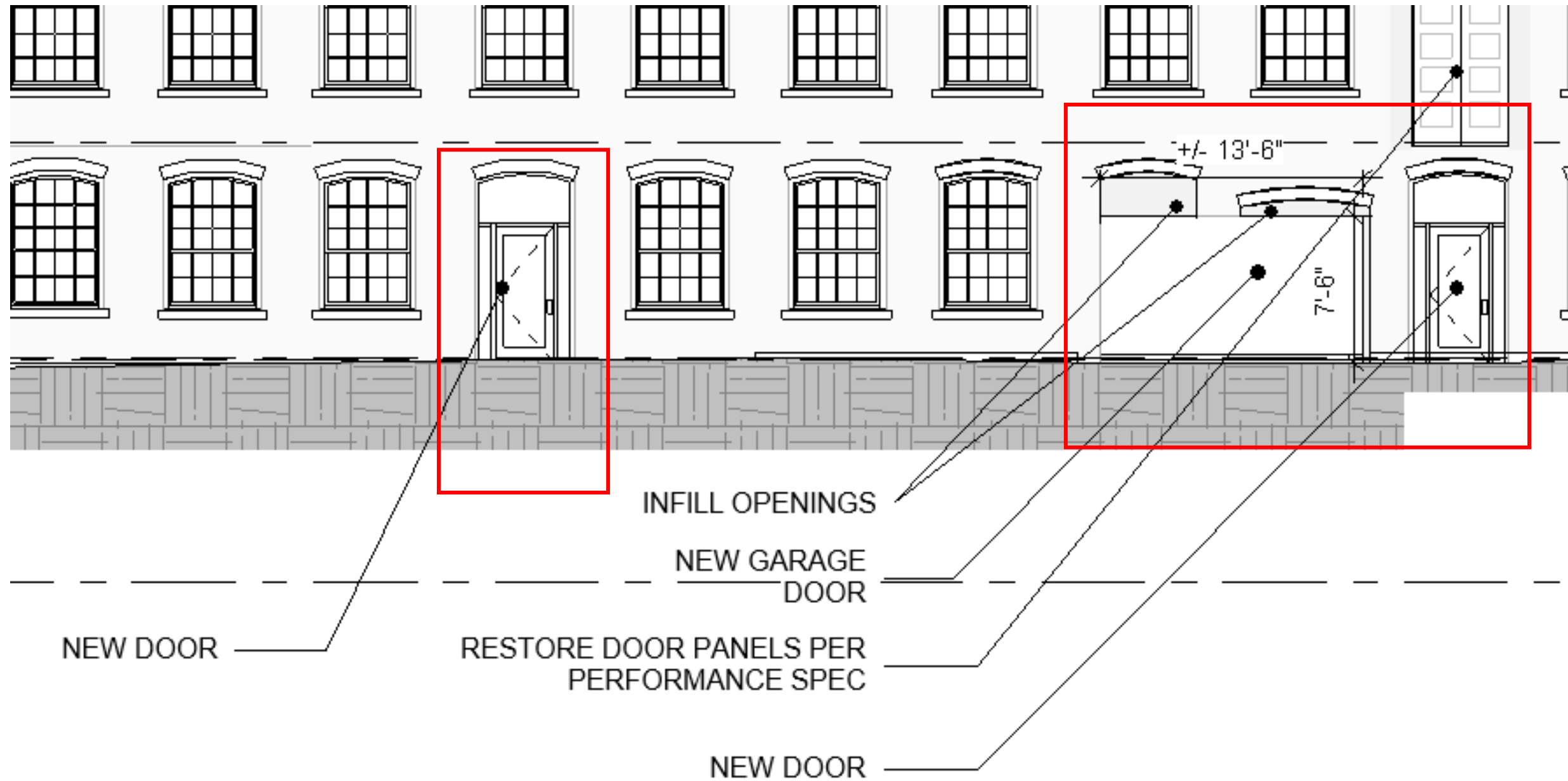
REVISED AREA
ADDED EXITS AND
GARAGE DOOR, SEE
PAGE 6



NEW
STOREFRONT

RESTORE DOOR PANELS PER
PERFORMANCE SPEC

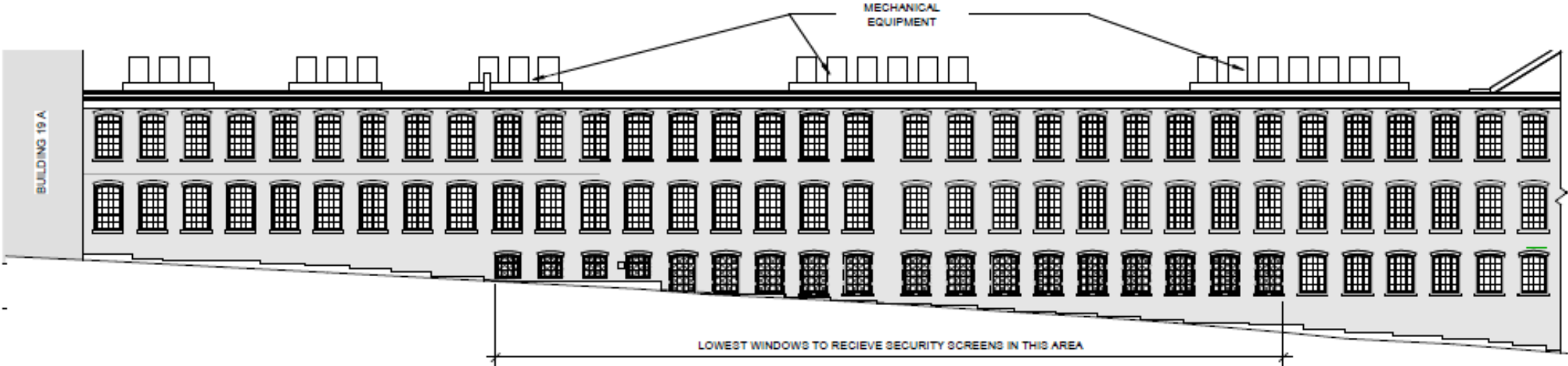
APPROVED BY PLANNING
10/9/2024



PROPOSED REVISION

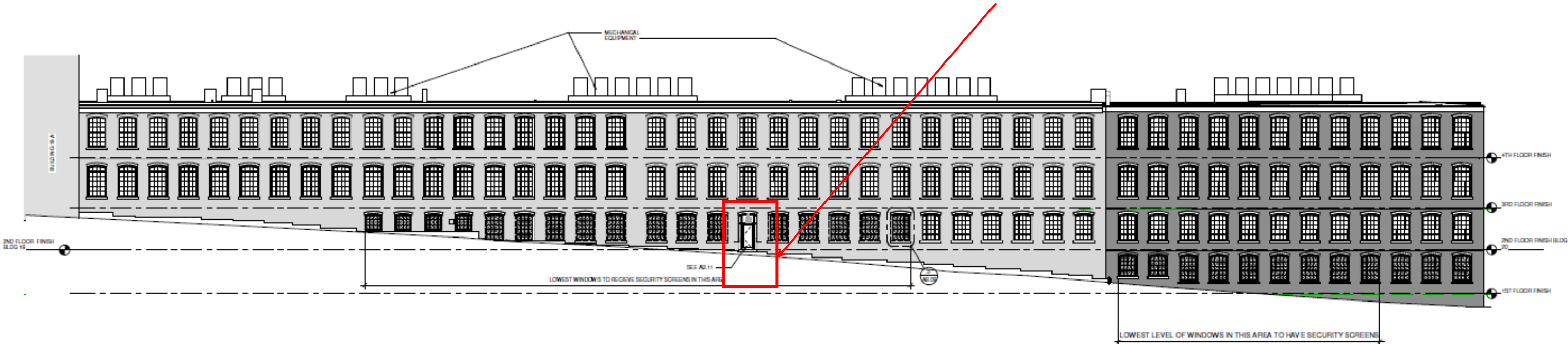
11/14/2025

Building 19 – SOUTH ELEVATION



APPROVED BY PLANNING
10/9/2024

PROPOSED ENTRY



PROPOSED REVISION
11/14/2025

MATERIALS



Floor tile in recessed entryway
Traktion Stowe Silver Dune
15x30 Matte Porcelain Tile or
similar



Recessed entryway wall tile
Daltile SlimLite Coastline
Fog or similar



Curved gypsum ceiling
painted SW 7017



Alcona Pendant or similar

EXISTING CONDITION



PROPOSED MAIN STREET ENTRY



PROPOSED GARAGE DOOR OPENING AND STOREFRONT ENTRY



Storefront System - AGS black anodized finish



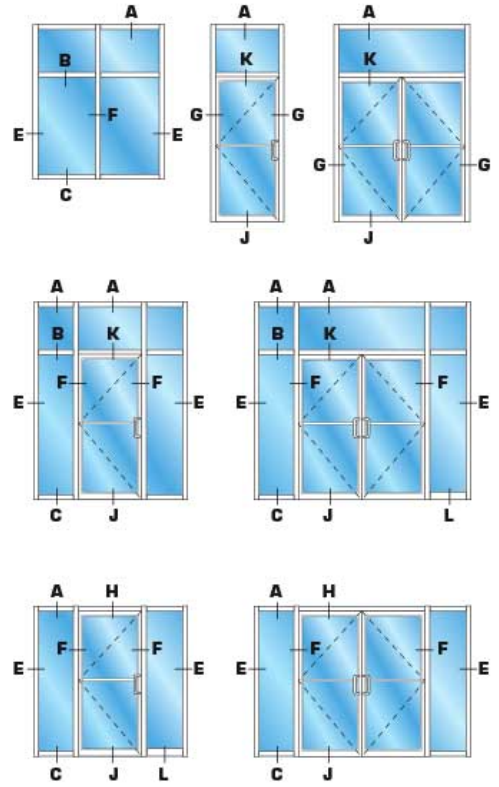
2" x 4-1/2" Flush Glazing
With Thermal Break



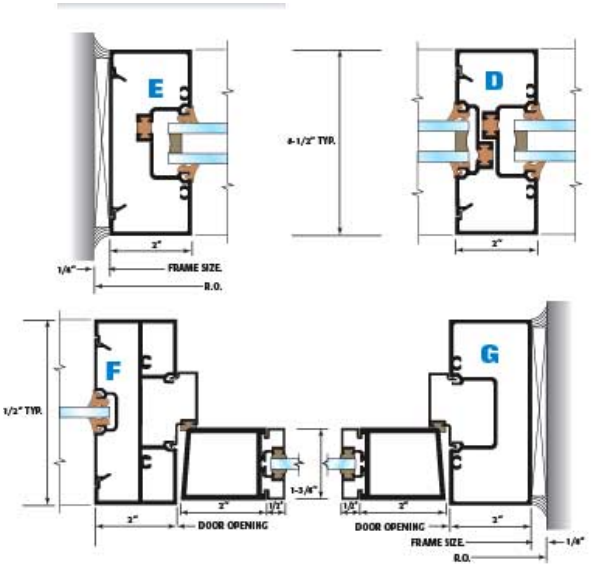
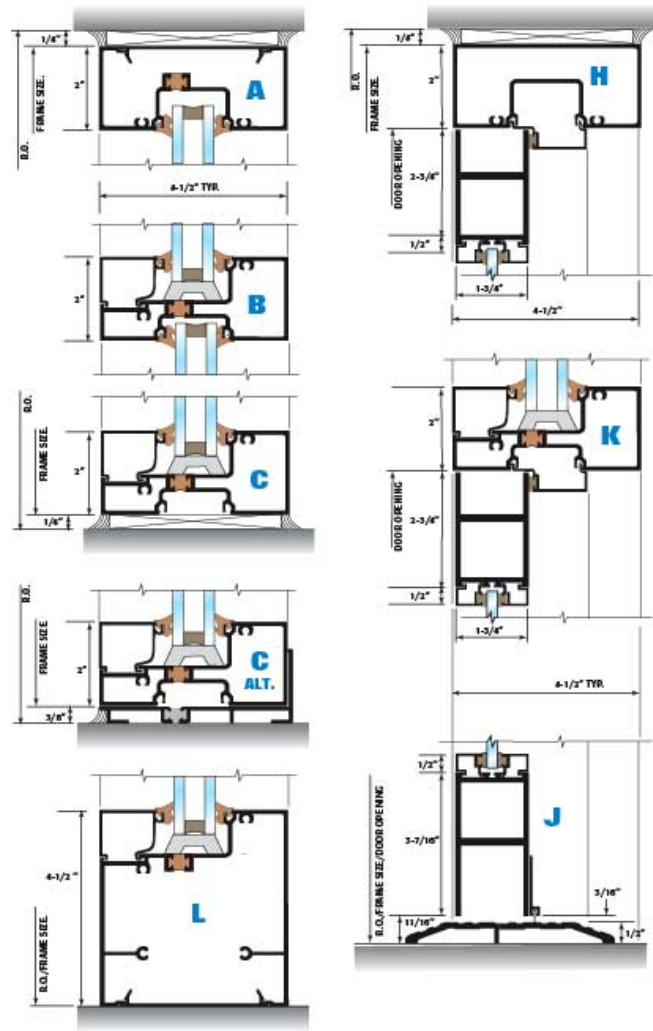
AGS

WINDOWS ENTRANCES PACER 450 STOREFRONT CUSTOM FAB METRA

2" x 4-1/2" Flush Glazing for 1" Glass shown with Thermal Break and Offset Door



2" x 4-1/2" FLUSH GLAZING FOR 1" GLASS
Shown in Thermal Break Version with Offset Door



300 SERIES INSULATED ROLLING DOOR – FINISH TO MATCH STOREFRONT FRAME

<p>MOTOR SPECIFICATIONS:</p> <p>2 HP motor to include an open drip proof motor, operated by a motor mounted Variable Frequency Drive and separate external NEMA 4 plastic enclosure control box. Worm gear reduction with built-in inertia safety brake and electrical solenoid brake with auxiliary chain operation. UL listed motor drives and control panel. Absolute encoder limit with power failure memory built-in. Guide mounted plug and play connections for sensors. High voltage leads pre-wired to motor and control panel. On board LCD screen to provide setup menu and troubleshooting messages. Class T fuse protection ready. Three button push station on panel with five button navigation for LCD setup menu.</p> <p>CURRENT CHARACTERISTICS:</p> <p><u>460 V</u> <u>3 PH</u> <u>60 HZ</u> <u>5.7 FLA</u></p> <p>Electrical current must be verified in writing before job is released for manufacture; current verified and found correct. Operator bracket bracing required, unless operator is wall mounted. Signed:</p>	<p>ELECTRICAL EQUIPMENT LIST:</p> <p>1 - Pair of Telco failure and function photo eyes. 1 - Telco failure and function light curtain. 1 - Brink Alert Kit w/ DSK 5-2. 1 - Motor Heater.</p>	<p>BOTTOM BAR LOCKING:</p> <p>None</p>	<p>QUANTITY:</p> <p>(1)</p> <p>MATERIAL & FINISH:</p> <p>Front Slat - Galv steel, phosphatized, CycleShield™ powder coating, color Verify powder Back Slat - Galv steel, phosphatized, CycleShield™ powder coating, color Verify powder Bottom Bar - Struc steel, CycleShield™ powder coating, color Verify powder Guides - Struc steel, CycleShield™ powder coating, color Verify powder Hood - Galv steel, phosphatized, CycleShield™ powder coating, color Verify powder Plain Steel - CycleShield™ powder coating, color Verify powder</p>	
<p>This item includes a 500,000 cycle warranty</p>				
<p>"SUPPORT TO RUN TO TOP OF COIL"</p>				
<p>ELEVATION (COIL SIDE) AND SECTION VIEW</p> <p>See drawing # <u>E 02110522 001 B</u> for guide detail.</p>				
<p>08-11-2025 MTR_STDR</p>	<p>PRODUCT CODE: MFW 6F 24GC3</p>		<p>MODEL #: EPI300 DEALER: CORNELLCOOKSON A.D.S.</p>	
		<p>CORNELL Innovative door solutions.</p>	<p>GFA MOTOR OPERATED ROLLING DOOR JOB: PEPPERELL MILL BIDDEFORD, ME</p>	
<p>— ORIGINAL DRAWING 09/16/25 CHRISS</p>		<p>* "Dealer" refers to any purchaser, customer, or reseller of CornellCookson products, and not necessarily an authorized CornellCookson dealer.</p>		
#	REVISION	DATE	BY	ARCHITECT: ARCHETYPE ARCHITECTS



Subject: Adding Vehicular Entrances and Garage Doors to Historic Buildings

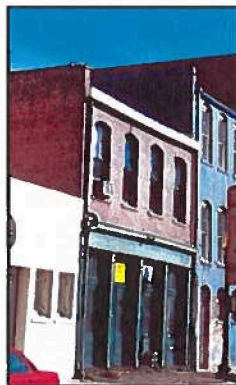
Applicable Standards: 2. Retention of Historic Character
9. Compatible New Additions/Alterations

Issue: Rehabilitating historic buildings sometimes requires using part of the interior for parking. Clearly, such an alteration can have a major impact on a building that was never intended for such use. It can also result in loss of historic fabric if it requires cutting an opening for a garage door. Thus, for most historic buildings, this is not a compatible rehabilitation treatment.

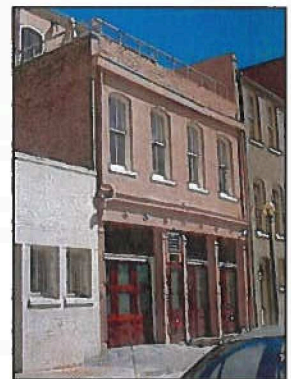
However, there are some historic buildings in which it may be possible to convert a portion of the interior for parking without negatively impacting their historic character. When considering modifying an interior space for parking that has not been used for parking previously it is necessary, first of all, to evaluate the character and condition of the interior space. Generally, only some industrial or commercial spaces or previously altered spaces which are basically devoid of character-defining features and finishes may be suitable to adapt for parking. If a historic building interior meets these criteria, the exterior must also be evaluated to determine if a garage door can be added without a significant loss of historic building material and without adversely impacting the character of the exterior. In most instances, a garage door may be added only on a secondary elevation. Selecting a traditional design for the garage door(s) in keeping with the historic period and architectural style of the building is also critical.

Application 1 (Compatible location/Compatible treatment): This mid-19th century building was originally used as a warehouse on the first floor with the owner living above. The building is narrow and very deep, extending through the block from one street to another. The primary elevation with its cast-iron storefront faces a busy street while the rear faces a smaller street that is more like an alley in use and appearance. The shipping entrance at the rear was separated by decorative cast-iron piers into four bays of double doors.

The rehabilitation project proposed to return the upper floors to residential use. The first floor, which had little historic features or finishes remaining, was to be converted into an office in the front and parking in the rear. The rear of the building is a secondary elevation and using this portion for parking was determined to be compatible. Since no garage opening existed, the rear entrance had to be modified for vehicular access while retaining its historic character. One cast-iron pier was moved over several feet to create an opening wide enough for a car. Wood garage doors replicating the existing pedestrian doors were installed. Although moving the pier resulted in a slight change to the formerly symmetrical entrance, it did not negatively impact its historic character. The project met the Standards.



Left:
Before rehabilitation
the ground floor of
the rear elevation
was divided into
four equal-sized
bays.



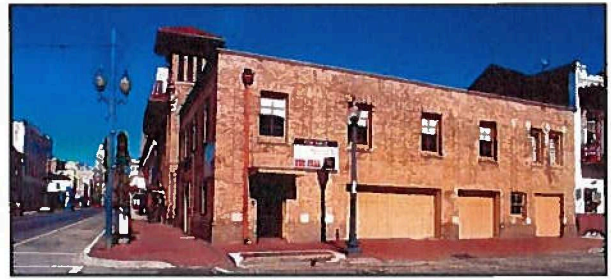
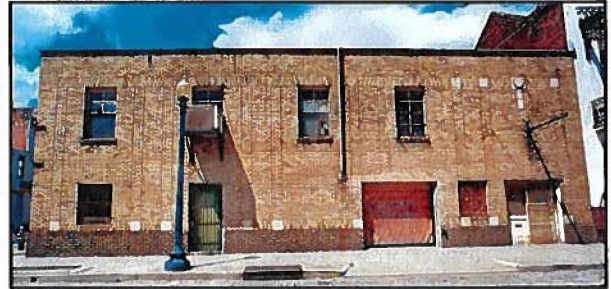
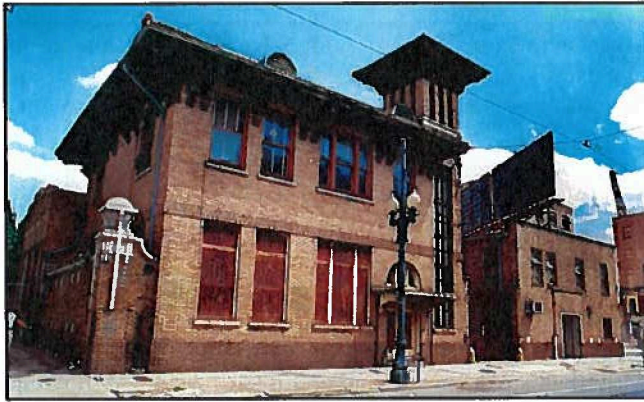
Left and Upper Right:
The left bay was
enlarged to create a
vehicular opening
which is compatible
with the building's
historic character.



Application 2 (Compatible location/Compatible treatment): This Tuscan Revival-style building was constructed in 1912 as regional headquarters for a national company. The property was to be rehabilitated for apartments. The first floor of the more utilitarian wing added later, which had been used for parking company vehicles, was proposed to be used as parking for the residents. Although one garage door already existed on the side of the addition, another, slightly wider opening was needed to allow incoming and outgoing cars to maneuver around cars parked inside.

Since the property is situated on a corner, this side of the building is clearly visible. But, it is a secondary elevation and of considerably less importance than the primary façade. **In this case, the existence of a garage door on this elevation was a factor in determining that adding another garage door would not greatly change the character of the historic building. A simple opening was cut into this wall and traditional paneled wood garage doors were chosen to complement the early-twentieth century style of the building.** The completed project met the Standards.

Below: A wing was added to the right side of the headquarters building several years after it was constructed in 1912. Because this wing had always been used for parking and had a garage door on the side elevation (right top), adding another garage door was determined to be a compatible treatment (right bottom).

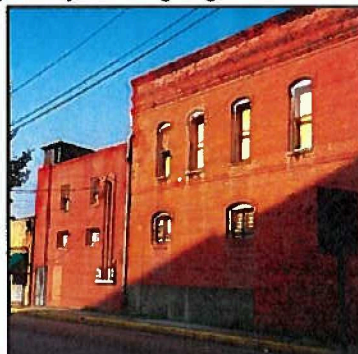


Application 3 (Compatible location/ Incompatible treatment): This 1880s retail building, situated on a downtown corner, had been altered numerous times since its construction. The storefront had been remodeled and a rear addition had been constructed. The first floor of the building was to be rehabilitated for continued commercial use and the second floor for apartments. The rehabilitation included a compatible storefront design, and a proposal to cut a vehicular opening in the side wall of the non-historic addition so that the interior could be used for parking. Although the rear addition faces a street, it is a secondary elevation, and it was determined that adding a garage door in this location would not negatively impact the character of the historic building.

Most treatments were completed as proposed. However, the new garage entrance differs from what had been proposed and approved. Two doors, varnished rather than painted as they would have been traditionally, are installed in a very large opening. A large pent-roof overhang which spans the entire length of the addition, extending from the garage to the back door, is also incompatible with the building's historic character. The garage entrance treatment did not meet the Standards and the project was denied certification.

Left and Center: The front and side elevations of this building are shown here before rehabilitation.

Right: The cumulative effect of the size, design and finish of the new garage door and the new pent roof overhang is a contemporary treatment that is incompatible with the historic character of the building.



Anne Grimmer, Technical Preservation Services, National Park Service

These bulletins are issued to explain preservation project decisions made by the U.S. Department of the Interior. The resulting determinations, based on the **Secretary of the Interior's Standards for Rehabilitation**, are not necessarily applicable beyond the unique facts and circumstances of each particular case.

November 2004, ITS Number 29



ITS
NUMBER 17

Interpreting The Secretary of the Interior's Standards for Rehabilitation

Subject: Adding Parking to the Interior of Historic Buildings

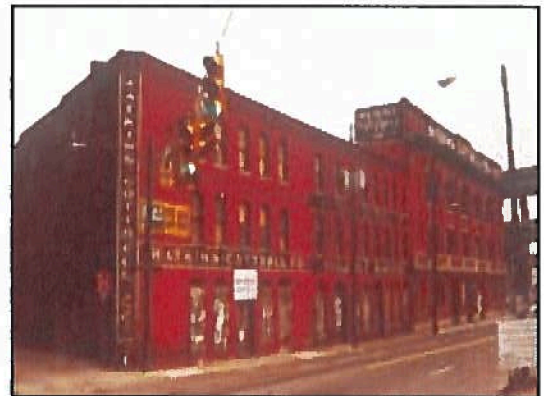
- Applicable Standards:
1. Compatible Use
 2. Retention of Historic Character

Issue: The rehabilitation of historic buildings in crowded downtown districts often creates the need for extensive new parking. City governments may have strict requirements for the number of cars a project must accommodate, a need never anticipated when most historic buildings were constructed. This forces property owners to investigate alternatives for meeting these demands, including adding parking to the interior of buildings. Although inserting parking into a historic building is generally not a recommended treatment, in some instances, parking may be introduced in a manner that meets the Secretary of the Interior's Standards for Rehabilitation.

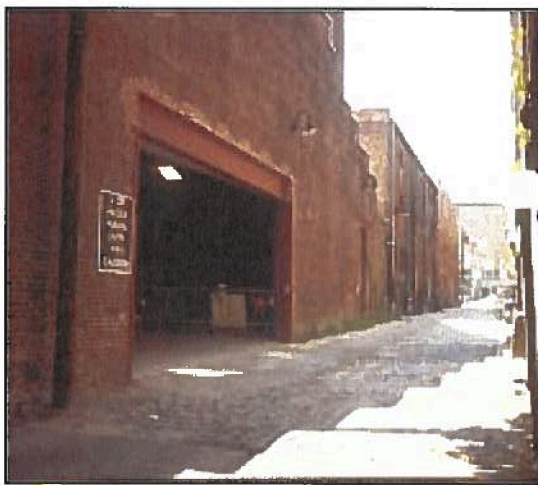
Parking inside historic buildings must not require extensive new openings on significant facades, and must provide adequate ventilation without disturbing the appearance of the existing fenestration. Mechanical ventilation is generally preferable to removing louvers or windows. The parked cars must also be screened from public view.

Application 1 (Compatible treatment):

When this late-nineteenth/early-twentieth century brick warehouse complex was converted to office and retail use, the local jurisdiction required the owner to provide a large amount of parking. Although the space designated for parking was not originally intended for this purpose, its utilitarian character lent itself well for this function and its warehouse quality, complete with its heavy-timber trusses, was left exposed. The entrance to the parking area was located along an alley, where existing openings were combined for vehicular use.



This brick warehouse complex built between 1865 and 1905 was converted to commercial and retail uses. A concealed rear structure worked well for interior parking.



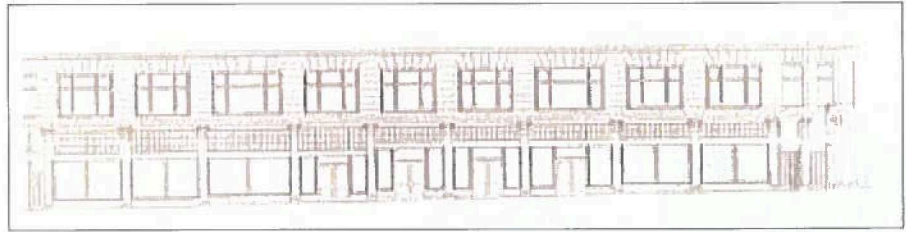
The new parking entrance is appropriately located along an alley where two smaller openings were enlarged to accommodate vehicular traffic.



The historic heavy-timber trusses were left exposed in the parking area.

Application 2 (Compatible treatment):

This early-twentieth century commercial structure in a major downtown area was converted into retail and residential use. Parking was introduced at the rear of the first floor, which had little historic fabric remaining, and in the basement, which was service-oriented in nature. The new parking entrances were added to secondary facades and were not visible from the primary elevations.



The transom of this compatible replacement storefront contains both spandrel glass and screening. The screened portion is used for ventilation.

In this example, it was difficult to ventilate to the roof, so other alternatives were investigated. The solution was to direct the exhaust out through and above the retail space. A modest transom detail was developed as part of the new compatible replacement storefront, which alternated spandrel glass with a dark screening material. This screening provided the necessary ventilation, without being too visually intrusive.

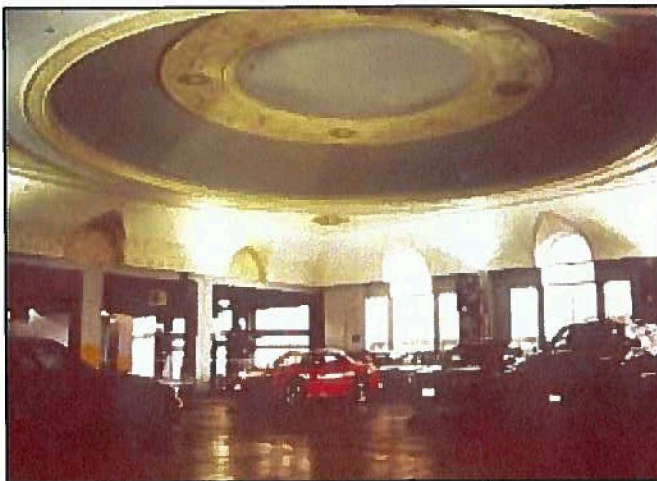


The parking area in this early-twentieth century structure is not visible to the public.



Detail of transom (under construction).

Application 3 (Incompatible treatment): Parking cars is never appropriate in historically finished interiors, as demonstrated by the following example:



Parking cars in this highly-finished space is not a compatible treatment.

Audrey Tepper, Technical Preservation Services, National Park Service

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September 2000, ITS Number 17



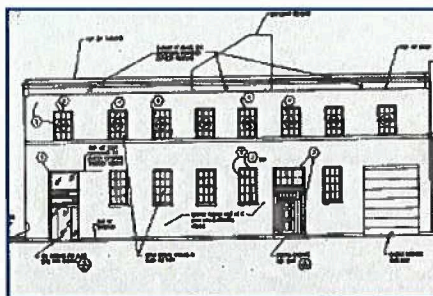
Subject: Adding New Entrances to Historic Buildings

Applicable Standards: 2. Retention of Historic Character
9. Compatible New Additions/Alterations

Issue: The rehabilitation of a historic building may sometimes require the addition of another or a second entrance on a primary facade, or the introduction of an entrance on an elevation that historically did not have one. Another entrance is most commonly needed when the building will have multiple uses after rehabilitation, for example, commercial or office use on the first floor with apartments upstairs, for which a separate entrance may be required for the residents. A new entrance may also be needed on what was originally a secondary elevation but which has assumed greater importance over time or with the new use.

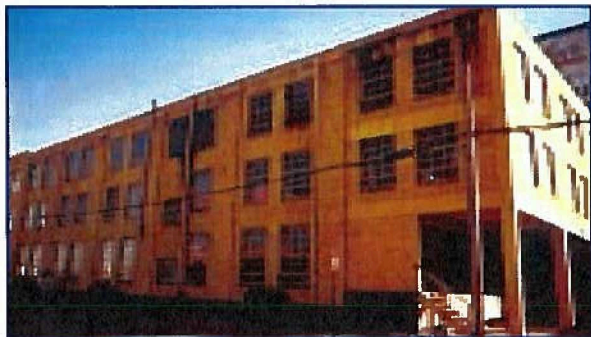
Generally, to meet the Standards, a new entrance should be simple in design; it should not appear historic; it should blend in with the historic facade; and it should be unobtrusive and modestly scaled. Adding a new entryway on a secondary elevation of a building should not give that elevation excessive prominence, nor should it 'reorient' the building or detract from the historic entrance. In other words, the historic front of the building should still read clearly as the primary entrance. Although it is always preferable that a new entrance be added to a rear or side elevation, in some instances a new entrance may be added on a primary elevation in a manner that is compatible with the character of the historic building.

Application I (Compatible treatment): This two-story, eight-bay masonry structure was built in 1886 as an ice manufacturing plant. Originally constructed with only one entryway, a garage door had been added later when the building served as a warehouse. As part of the building's conversion into offices, a second pedestrian entrance was added to the street elevation during the rehabilitation to make it easier to get to some of the offices. The size of the new opening is the same as that of the existing historic entrance. But, the new entrance is almost entirely glazed, and consists of a simple butt-mounted glass door with sidelights, and a single-light transom. It is clearly a compatible, contemporary design that does not draw attention to itself. It cannot be confused with the historic entrance, and it does not change the character of the building. Thus, it meets the Standards.

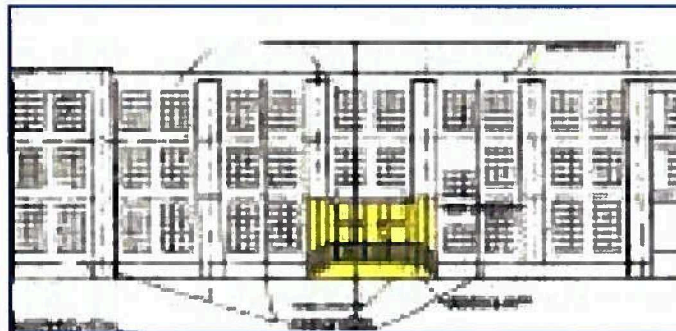


This building was constructed with a single pedestrian entrance in 1886, and a garage door was added later (left). When the building was rehabilitated for office use, an existing window was removed from the end bay and replaced with a new glazed entryway (center and right).

Application 2 (Compatible treatment): A larger, free-standing, three-story warehouse building constructed in 1922, with a 1940s addition, was to be rehabilitated into commercial and retail spaces on the first floor with residential apartments on the upper floors. The building featured a loading dock on one side and three utilitarian, non-significant entrances on various elevations. As part of the rehabilitation a new entrance was proposed to be added on a side of the building that never had an entrance. An entrance on this elevation would improve visibility and access to the new shops and businesses, and it would also help increase security for the upstairs apartments since existing entrances could be restricted for residential tenant use. Accordingly, a new glass and steel entryway which reflects the industrial character of the building and its historic metal windows was designed for this side of the building. **The new entrance is compatible with the character of the historic building. It is unobtrusive and it does not noticeably impact or change the appearance of this elevation or of the warehouse building as a whole.**



This historic warehouse had entrances on three elevations of the building prior to rehabilitation.



A compatible, new entrance was added to the fourth side of the building during rehabilitation.

Anne Grimmer, Technical Preservation Services, National Park Service

These bulletins are issued to explain preservation project decisions made by the U.S. Department of the Interior. The resulting determinations, based on the **Secretary of the Interior's Standards for Rehabilitation**, are not necessarily applicable beyond the unique facts and circumstances of each particular case.

October 2001, ITS Number 22